

Australian Model Railway Association

JOURNAL

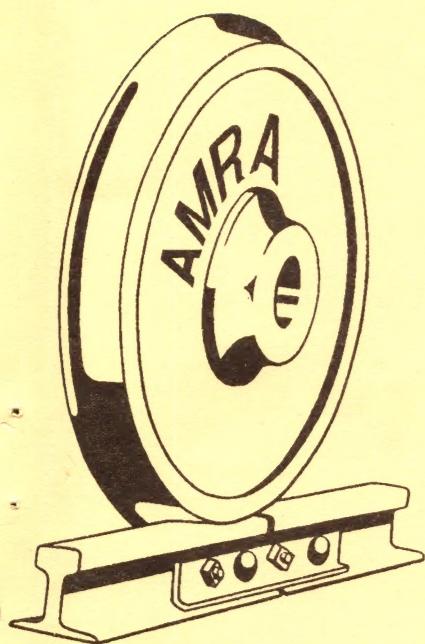
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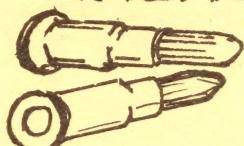
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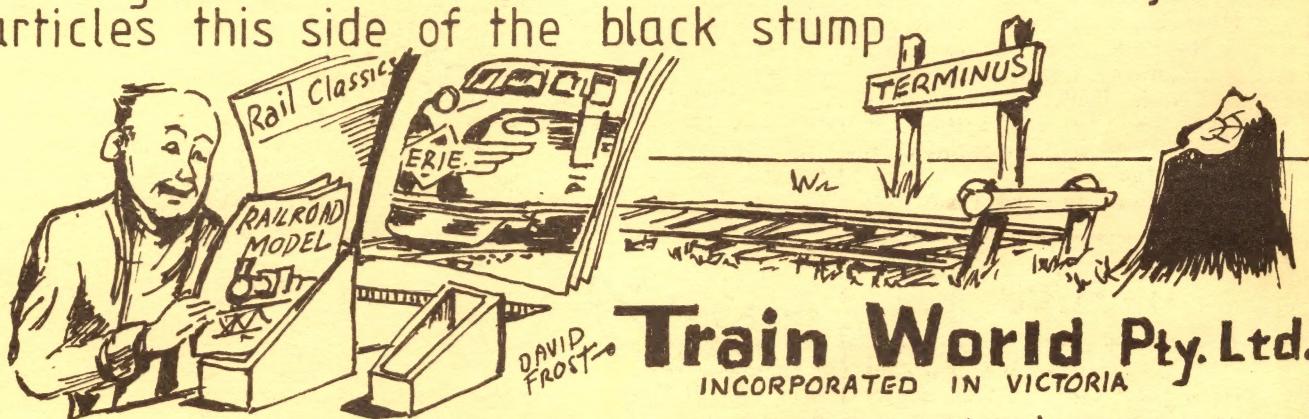
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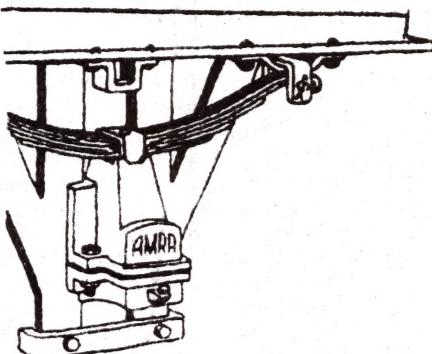
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Editorial

JOURNAL BOX

From the Managing Editor

One of the attractions of the hobby for those of us who are beyond the young adult stage is the ability to recreate in miniature some of the atmosphere and nostalgia surrounding railways when we were children.

My one lasting regret is that in my late teenage years, I became more interested in the allure of the opposite sex and other such distractions and thus did not spend more time recording the scene as was then, at that time, the transition from steam to diesel.

Roger Lloyd

On the Cover**A Scene Seldom Modelled - Times of Change**

This scene taken in the early 1970's shows QR 1200 class No 1207 with a goods train ready for departure northwards at Rockhampton, Central Queensland, in a period of disarray during the process of demolishing the old Rockhampton station. The new station is a kilometre down the track on the right. The old station's arched roof has recently gone and the tracks in the foreground lead nowhere - like some model railways? The train behind 1207 is mostly wooden wagons, these would all no longer exist, along with 1207, an English Electric Locomotive built in the U.K. in the 1950's. All have now been cut up. One Railway Historian commented to me one day. "The only way we can relive Historic Railway scenes is in model form". "Model Railways need not be 'Boys with toys', but a 3D artform, in a very real way, not appreciated by society at large".

Photo by John Elsol 1972

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Editorial Deadlines

The deadlines for the next issue will be 23rd June for hand written articles, 28th June for neatly typed articles and State News, 30th June for articles on floppy disk (IBM format) and 14th July for addresses and envelopes. Collating and posting is expected on 19th July.

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Secretary's Desk.

Not much to say this time. This part of the year is somewhat like the full before the storm. The way that the AMRA year works out the Federal Committee get really busy in July with plans for the next AMRA year, followed by membership renewals, the AGM, lots of letters, many decisions that need to be made, and so on, and so on.

Congratulations to Fred Stell from NSW our final Meritorious Award winner for 1992.

A WARNING for members travelling overseas. Some of you may be thinking of buying video tapes while you are away. There are three different television systems in use in the world and it is not enough to check that the tape is VHS or BETA, you also need to check that the tape is for the appropriate TV format. Australia, the UK, and parts of Europe all use the PAL system so this is the format that you will need to buy for use here. The USA and JAPAN use the NTSC system and France and some other parts of Europe use SECAM. A VHS tape from the USA that is in NTSC format will play in your machine once you get home and you will be able to hear the soundtrack BUT you will not be able to get the picture. Tapes in these other formats can be converted

Continued next page

but it is easier to buy the tapes in the right format (if available) to start with. Many tourist places around the world have tapes available in the various formats all clearly labelled with the format (and language). Look for tapes labelled PAL. If in doubt, ask.

Stephen J Chapman
Federal Secretary

Notes from the Registrar

Be warned that in the next issue of JOURNAL July/August will be found the Renewal and Nomination Form. If you intend renewing fill in the Renewal Section and return to me, INTACT. The Nomination section if used send to the Federal Secretary.

Also if you are due for Renewal, the Address Label will be marked SEP 1. If there is no date mark no money is required. For some reason quite a few members doubled paid for this year. Makes me wonder what some do with the membership card that gets sent back around a week after their subs are received as the card always shows financial to 1st Sept 93 or whatever year applies.

I could allow for a few cases where the form does not get inserted, but I strongly doubt that the number who claim that no form was in their JOURNAL is true. One member claimed there was only a

nomination form in his issue ???

There is a lot of time and effort expended during the Renewal period and it does help if members do co-operate and do the right thing. At present the membership stands at 1034 and of that number some 50 odd have changed their address since 1st September, a couple twice, and after the Address Labels have been printed and posted to Melbourne, there is always a last minute FAX or night STD call for last minute changes. The saying is no peace for the wicked, but it is some 15 years since I last laid hands on the 'Castle'. At least being of brass construction it is not rusting away, although I have noticed a bit of rust on the bogie wheels!

Norm Read
Federal Registrar

The Managing Editor

Dear Roger,

I received the last issue of "Journal", March/April, and turned eagerly to the members file, hungry to see my name in print, only to be bitterly disappointed to find that you have overlooked me completely, while including the name of a friend who joined at the same time as I. Boiling with rage, my thoughts alternated between suicide and bombing your editorial offices, until I finally hit upon the perfect solution. I decided to write to you and give you a chance to redeem yourself, by publishing a correction of page 1-3-9 in the next issue, together with an abject apology. This is also an ideal moment to notify you of my change of address (address supplied) (not that this is any excuse for forgetting me!).

Yours faithfully,
(the slighted) Alex Danilov

Dear Alex

I will give you an abject apology for dropping your name off the membership list if you will give me an abject apology for sending your change of address to me rather than to the Federal Registrar, to whom I have since forwarded your letter! I trust this will help curb your violent tendencies!

Managing Editor

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Diode Matrix Turnout Control

by Stephen J Chapman

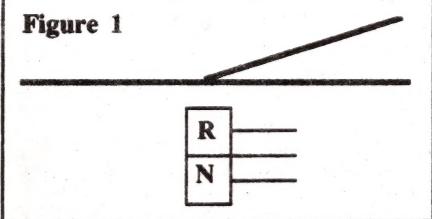
To a modeller who is not very interested in the electrical aspects of a model railway, a diode matrix is a complete mystery. The purpose of this article is to remove some of that mystery and explain what a diode matrix does and how to create one.

Let us start by considering what a diode matrix is used for. A diode matrix is a part of the circuitry that can be used with electrically operated turnouts. Without the matrix the operator would have to throw each turnout along the selected route individually. With a diode matrix you can set up the complete route at the press of a single button.

Route control is straightforward using a diode matrix. To achieve the same result using some other method is more complicated. One of the advantages of having electrically, rather than mechanically, operated turnouts is that it allows route control to be added in such a simple way.

To see how simple a diode matrix really is, let us consider a few simple track plans and see how they would need to be wired so that all of the turnouts on a given route can be set appropriately. The simplest track plan that we can consider is a single turnout (figure one). Here we have two possible routes, one that uses the straight road through the turnout, and the second uses the curved road. The point motor attached to the turnout has two solenoids built into it which pull the points to either side to set the appropriate road. Let us call the solenoid that sets the straight road the Normal solenoid (N) and the one that sets the curved road the Reversed solenoid (R). To wire this up we must take one wire from the N solenoid to the N button and one wire from the R solenoid to the R button. The other wire from each solenoid goes to the common return. Now we can set the straight road by pressing the N button and set the curved road by pressing the R button. Simple and we didn't even need a diode matrix.

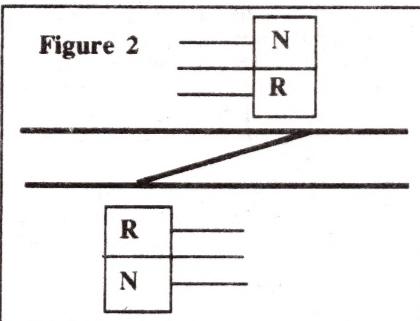
Figure 1



Next lets consider a couple of situations where we have two turnouts. The first is a simple crossover (figure two). Here we have three possible routes. We can go straight through on the top track, straight through on the bottom track, or follow the curved road of both turnouts to cross from one track to the other. With a little thought we realise that the position of

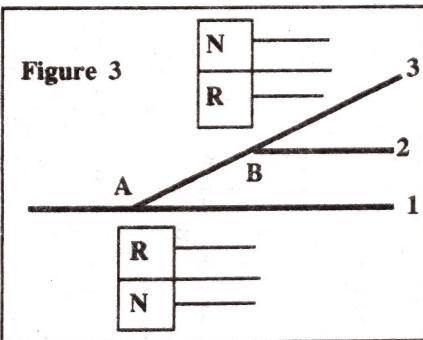
the second turnout doesn't matter when we are using one of the straight through routes unless we are using both straight through routes together. We can therefore consider the two straight through runs as one route setting. So again we have two possible route settings, one with both turnouts set straight, and the other with both turnouts set curved. We can wire both of the N solenoids to the same N button and both of the R solenoids to the same R button and have the two turnouts operate together to give us the routes that we require. Press the N button to set both straight through routes and press the R button to allow a train to cross from one track to another.

Figure 2



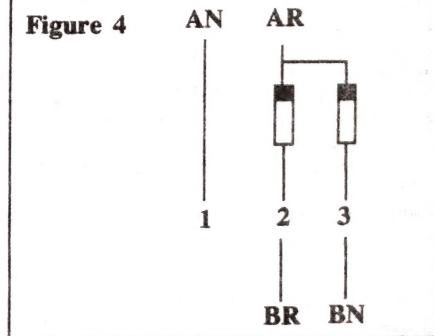
We still haven't needed a diode matrix and you're probably starting to wonder what we need a diode matrix for. This will become obvious when we consider our next track arrangement involving two turnouts (figure three). Here we have two turnouts arranged somewhat differently than in our last example. We have three possible routes each involving the single track at the left hand end of the diagram and leading through to one of the tracks at the right hand end. Let us label these routes 1, 2, and 3 so that we know which route is which. Let us also label the turnouts A and B. So route 1 links the left hand track via the straight road of turnout A to the right hand track labelled 1. Route 2 links the left hand track via the curved roads of both turnouts to the right hand track labelled 2. Route 3 links the left hand track via the curved road of turnout A and the straight road of turnout B to the right hand track labelled 3.

Figure 3



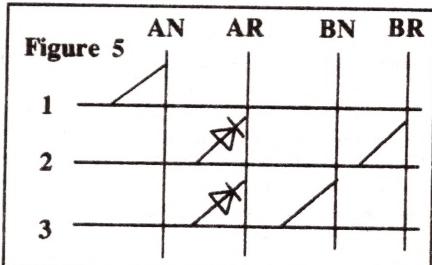
To wire up this combination we will require three buttons to correspond to the three routes. Button 1 will need to be wired to the N solenoid of turnout A. Button 2 will need to be wired to the R solenoid of both turnout A and turnout B. Button 3 will need to be wired to the R solenoid of turnout A and the N solenoid of turnout B. Now we have a problem. Both button 2 and button 3 need to be wired to the R solenoid of turnout A. This means that the two circuits are cross connected and pressing either button will supply power to the R solenoid of turnout A and both solenoids of turnout B. We need to find something to stop the power from circuit 2 getting into circuit 3 (and vice versa). The something that we can use is a diode, or rather two diodes. A diode only permits power to flow in one direction. If we wire a diode between button 2 and solenoid AR and another diode facing in the same direction between button 3 and solenoid AR (figure four) then we will have successfully separated the circuits again. The current which flows from button 2 through the diode to solenoid AR will not be able to get through the diode to button 3 and hence power will not reach solenoid BN. Similarly when power flows from button 3 via the diode to solenoid AR the other diode will stop the current reaching button 2 and hence power will not reach solenoid BR. The circuit now works correctly and the appropriate route will be set when each of the buttons is pressed.

Figure 4



So now we have a turnout circuit that requires diodes but we still haven't explained how this relates to those funny looking diagrams that we normally refer to as diode matrices. In fact what we have here with our two diodes is a very simple diode matrix. The wiring diagram for our track plan is shown in figure five in the normal format of a diode matrix. In a diode matrix we have one line down the page for each point motor solenoid (ie. two per turnout) and one line across the page for each route. Diagonal lines linking a route to a solenoid indicate that we need to run a wire between the button for that route and

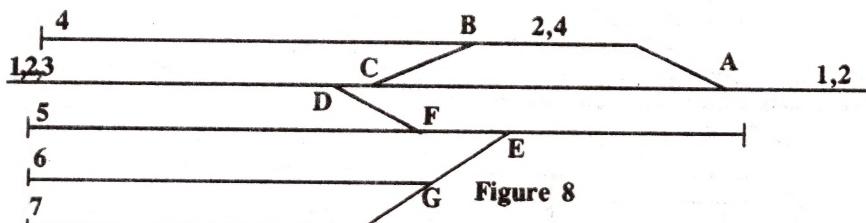
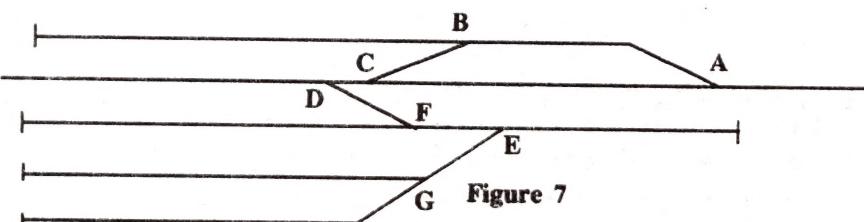
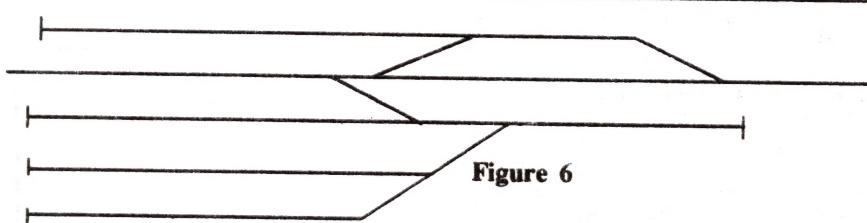
that solenoid. The little arrows with the bar across the end that appear on some of these diagonal lines indicate that a diode is required in that wire. A diode is required whenever more than one route button is connected to the same solenoid. A diode is required in each wire leading to that solenoid.



Let us now look at how we can develop a diode matrix for a more complicated track plan. The process of developing a diode matrix is the same regardless of the actual track plan so it doesn't matter which track plan that we use as an example. Figure six shows a simple station track plan involving a loop and a few sidings that we can use for the purpose of showing the steps involved in developing a diode matrix.

Step ONE. Draw your track plan. A diode matrix is meaningless unless we also have a properly labelled track plan that shows how the diode matrix relates to the layout.

STEP TWO. Label your turnouts (figure seven). We need to have the turnouts labelled on the track plan so that we can determine which solenoid labelled on the diode matrix relates to which track setting on the actual layout. For example solenoid BN on our as yet to be drawn diode matrix will refer to the turnout at the top of our track plan being set to the straight position.



STEP THREE. Label your routes (figure eight). In our earlier example we could label the routes simply by numbering the tracks at the right hand end. This was because the other end of each route was obvious. In this example we not only have to label each end of each route but we can tell where each starts and finishes (eg. routes 3,5,6,7) but we also need to mark the centre of each route where more than one route can start and finish at the same place (eg. routes 1,2). Each route should correspond to an intended movement on the finished layout so not all routes need necessarily extend through all possible turnouts (eg route 4 which allows a train to move between the loop and the siding but does not pass through turnout A onto the main line). In fact if you do not intend to ever run through a particular route then that route need not be marked on the diagram (eg. at a double track terminus having two crossovers in the approach you will never use the route that crosses through both crossovers).

This provides us with all the information that we will need in order to be able to relate the diode matrix to the layout. The next step therefore is to commence drawing the diode matrix.

STEP FOUR. Draw the basic grid for the diode matrix (figure nine). We draw one vertical line for each solenoid (ie. two per turnout) and one horizontal line for each route. These lines are labelled to match the way that we have lettered and numbered our track diagram.

STEP FIVE. We next determine which solenoids need to operate in order to set each route. As an example let us consider route 2 on my track plan. To set this route we need to set turnout D normal, turnout C reversed, turnout B reversed, and

turnout A reversed. We therefore draw diagonal lines linking the horizontal line for route 2 with the vertical lines for solenoids DN, CR, BR, and AR. Figure ten shows the way that our diode matrix looks once the diagonal lines have been drawn in for all of the routes.

STEP SIX. Check that we haven't left out any essential routes. We can check that at least one route passes through each leg of each turnout by checking that there is at least one diagonal line linking to each vertical line. If we find a vertical line which does not have a diagonal line attached to it the either we have made a mistake on our diagram or we have left out a route. Checking the diagram in this way does not check that you have included all of the routes that are possible for your track plan or even all of the ones that are desirable. It simply checks that all of the solenoids are included in at least one route and that each turnout will actually be useable.

STEP SEVEN. Draw in the diodes (figure eleven). Check each vertical line on your diode matrix again. Whenever there are two or more diagonal lines attached to it we need to draw in a diode (the reason for this was explained earlier).

So now our diode matrix is complete and ready to be wired. The diode matrix for your track plan probably looks completely different to mine but it will work in exactly the same way.

We can now wire up the diode matrix. As mentioned before a diagonal line indicates a wire running between the specified route button and the solenoid indicated by your labelled track diagram. A diode shown on the diagonal line indicates that a diode needs to be included in the wire. It is essential that all diodes be wired the same way around. Diodes are marked in some way to indicate which end is which. A common way of marking is a silver band around one end. Just make sure that all of the diodes are wired with the silver band at the same end all of the time. It doesn't matter at this stage whether the band is nearer the button end or the solenoid end as long as they all face the same way.

The next thing to consider is the power supply. Most controllers come with an auxiliary output intended to operate accessories such as point motors. This supply will either be a separate transformer winding with a very low power rating or will alternatively come off of the same winding as the train controller itself. It doesn't really matter which of these two is the case so you don't need to start worrying about which type you have.

Each of these types of supply is suitable for operating one or two turnouts quite satisfactorily. Each has problems when attempting to throw a large number of turnouts simultaneously as may be required with our diode matrix turnout control system. A controller having a separate wind-

ing will probably not have sufficient power to throw the points across properly. A controller running the turnouts off of the same winding as the train control will have enough power but only at the expense of drawing power away from the train and hence causing the train to momentarily slow down. Neither of these situations is desirable so we need to take some action to resolve this.

One solution would be to add a separate power supply for turnout operation. This can be quite expensive and is not really necessary. The key to solving this is the fact that we only need this large amount of power for a short moment while we are actually setting up a route and throwing the required turnouts. If only there was some way of storing up the small amount of power available from our controller in such a way that a large amount would be available when we need it. (A simple way of looking at this is to imagine a tank of water which is being filled slowly by having water dripping into it. When we need a lot of water we have a tankful that can be released all at once). The device that does this for us is called a capacitor discharge unit (CDU). By attaching a CDU between the power supply and the route buttons we will have all of the power that we need provided that we don't need to set too many routes to quickly. Now the direction that you have wired up your diodes becomes more relevant. A CDU, as well as storing up the current until we need it, also converts the current from AC to DC (ie. instead of the current flowing alternately in both directions it now only flows one way). The simplest way to sort out this problem is to wire in the CDU and try it (figure twelve). If it doesn't work then simply reverse the two output wires from the CDU. It should then work.

And that is all that there is to diode matrix turnout control. Of course if you have a very complex track plan it may be simpler to split the diagram into several sections and develop each section as a separate matrix. This would have the effect of requiring two or perhaps three buttons to be pressed to set up a particular route but with a large reduction in the number of route buttons required.

It is all a matter of picking a suitable level of compromise. With one big matrix you may be able to set any desired route at the press of single button but you have lots of buttons. Without a matrix at all you will still have lots of buttons and will have to press a lot of them to set up some routes. With a couple of smaller matrices you may require only a few buttons, only a few diodes, and be able to select any desired route just by pressing one or two buttons.

The diode matrix is a very useful means of simplifying the operation of your model railway. If you are using double solenoid point motors to operate your turnouts this enhancement is well worth considering.

Figure 9

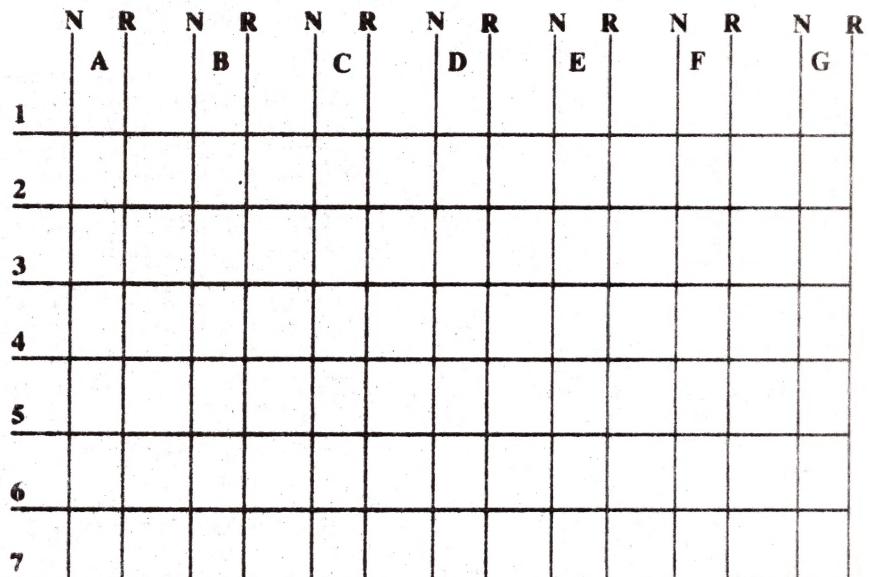


Figure 10

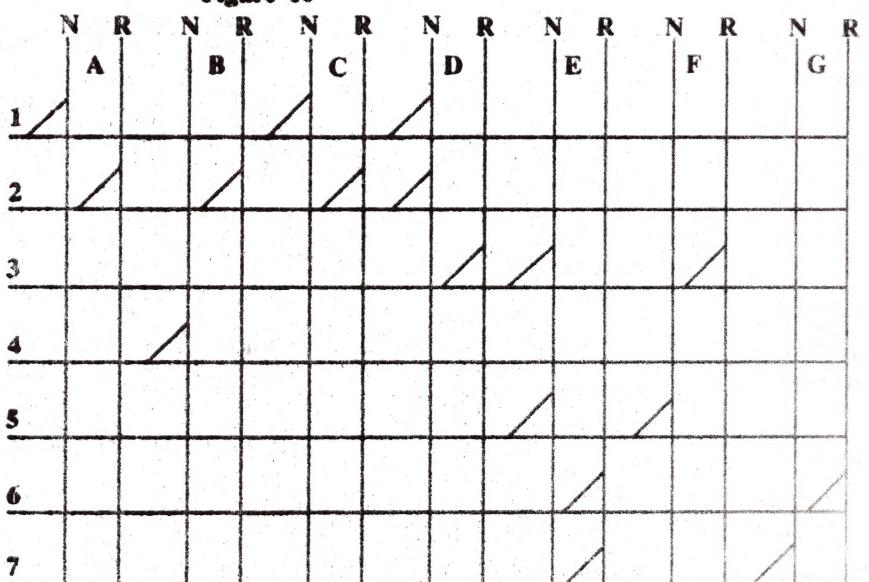
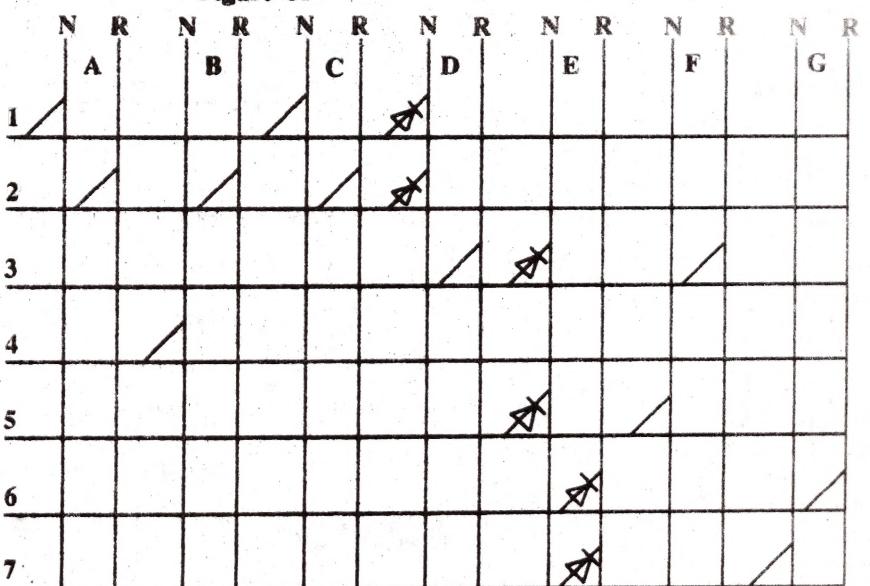


Figure 11



Additional Notes on L-Girder Construction

Tom Parkes

Further to the article on L-Girder construction by Steve J Chapman in the January-February 1993 issue of Journal (No 212), the same form of construction has also been used for a moderately large layout with the concept of being relocated someday in another area approximately 7.62 m by 3.67 m with facilities for an 8.5 m x 2.7 m extension. My layout plans have usually involved many pages of scale dimensions and from experience the selection of a mainline minimum radius is a basic starting point, particularly when combined with the squares system presented by John Armstrong in his book 'Track Planning for Realistic Operations'.

There are, of course, a large range of books on layout plans from the UK, the USA and the Continent, and plans are featured in many issues of model magazines, so 'rush in slowly' and plan carefully and check, check and re-check.

On the balance of probabilities, track changes will either be contemplated or required as layout construction progresses! Having decided on the track plan, consider the advantages of the use of easements into each curve and the possible use of super-elevation. The easements will give a smooth transition from a straight section into that minimum radius curve and combined with super-elevation may ensure that the 'someone' with a hi speed fixation will avoid derailments, and may also avoid the horror of that long slow drop from the track high point to a concrete based floor.

At this point, the 'authorities' may require: an environmental impact

statement; an economic comparison; a feasibility study; and if 1975-style economics is favoured, an assured proof of a 10% annual growth in income! In my case, there was a further slight delay while piers were replaced with RSJs, rock was excavated, walls were built, doorways were cut in brickwork and concrete was mixed and poured. (It may be that several hernia repairs and coronary artery grafts could have ultimately been related to the effort involved, but it was excellent for weight reduction and appetite enjoyment....).

The basic L-Girder cross section illustrated in Figure 1 of Steve's article was used in conjunction with a modular section 2000 mm long by 620 mm wide. Since the aim was to develop a relocatable layout, cross members were fitted at the ends and across the mid-point of each module. A circular saw was used to re-saw timbers on hand to 75 x 25 mm for the vertical section and 50 x 25 mm for the horizontal section of the L-Girders and when glued and screwed at approximately 300 mm intervals, the effective depth of 100 mm in clear Oregon was found to give quite adequate support for body weight at the mid-point of each module.

The cross members, or joists, to be fitted above the L-Girders were 75 x 25 mm in width and thickness and were varied in length to fit the dimensions of the layout plan. An increase to say, 100 x 25 mm is suggested when the overhang from the L-Girder exceeds 450 mm, particularly if 'body'

weight is likely to be applied at the outer end of the joist. Risers were cut from scrape joist and cross member sections, and the track base was fixed to the risers using 25 x 25 mm cleats screwed from the bottom of the cleat. Bear in mind that the track base fixed from the top can present difficulties if risers have to be relocated for special track features - removing screws when the head is under the track can be tedious!

The trestles used for the layout were based on the type used by the late Gordon Duncan in his modular layout system. The advantages are the simplicity of the 50 x 25 mm sections, assembly with four 6 mm bolts which, in fact, were 1/4 in coach bolts recycled from an earlier project. One section of the layout was fixed to a brick wall with a step 225 mm wide by 530 mm high, and a sketch illustrates the support system adopted since the standard trestle could not readily be adjusted to fit.

Track base curves were cut from 5/8 in plywood using templates (an alternative is to use a radius rod with the jigsaw fitted to the free end) and were then set out temporarily on the framework so that easements could 'flow' in, using splines, from the straight sections of track. The positions of the risers were then marked on the joists and, where necessary, joists were relocated.

If possible, aim for a mainline grade of no more than 2%. Incidentally, there is no longer any recall of the equation for a spiral, let alone the application, but the sketch does illustrate the development of a simple

system to produce an easement that will be found to be all that is required for layout applications. do recognise that particle board will 'grow' in damp locations unless the waterproof variety is used and that it will require more support than plywood, redwood or cedar. Incidentally, cedar was found to be an excellent medium for track base. It is easy to cut, is very stable, reduces track noise and, in my case, was without cost.

Fix the risers to the track base using 25 by 25 mm cleats screwed to the track base from the bottom and then fix the risers to the joists. A small clamp was used to set the risers at the correct height above the joists and also in applying super-elevation to the mainline curves. Fitting the cork track bed to the track base will be simplified by cutting strips approximately 22.5 mm wide and butting the strips to the track centre line. PVA glue slightly weakened with water is an adequate adhesive for the cork strips which may be secured temporarily with office-type staples and/or weights. The second strips are then butted

Material List and Dimensions for L-Girder Modules and Trestles

Module			
Length	= 2000mm	Width	= 620mm
L-Girder	Horizontal = 50mm x 25mm	Vertical = 75 mm x 25 mm	
	End piece and cross member = 75 mm x 25 mm x 570 mm		
	Joist = 75 mm x 25 mm by length to fit layout plan		
	Riser = 75 mm x 25 mm by height to fit track plan		

Trestle

Leg	= 50 mm by 25 mm by 1000 mm by 2 = 4 per trestle
Top cross member	= 50 mm by 25 mm by 620 mm 1 per trestle
Bottom cross member	= 50 mm by 25 mm by 800 mm 1 per trestle
Brace to L-Girder	= 50 mm by 25 mm by 1200 mm 4 per alternate module

Track Base

Curves were cut from 16 mm plywood

Straights were cut from 16 mm particle board or from 12 mm cedar or redwood offcuts
Station and yard areas were from 16 mm plywood/particle board

Track Bed

Cork strip 22 mm wide from 3 mm sheet 2 per track

Hardware

Coach bolts = 6 mm x 100 mm 4 per free standing trestle
2 per wall trestle

Coach bolts = 6 mm x 75 mm 4 per module end

Angle bracket = 100 mm x 100 mm x 50 mm 1 per wall trestle

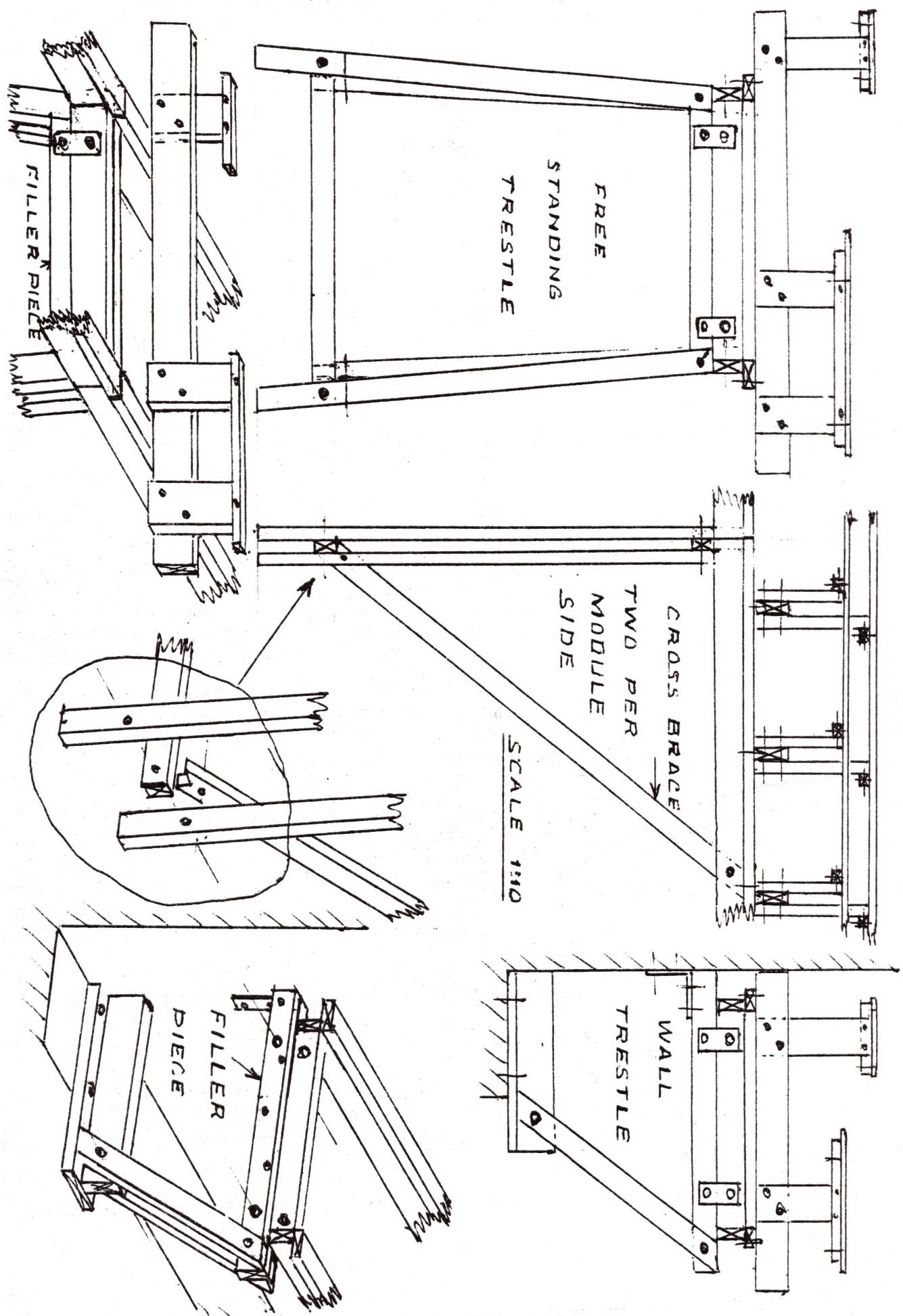
(To fix upper section of wall trestle to masonry wall)

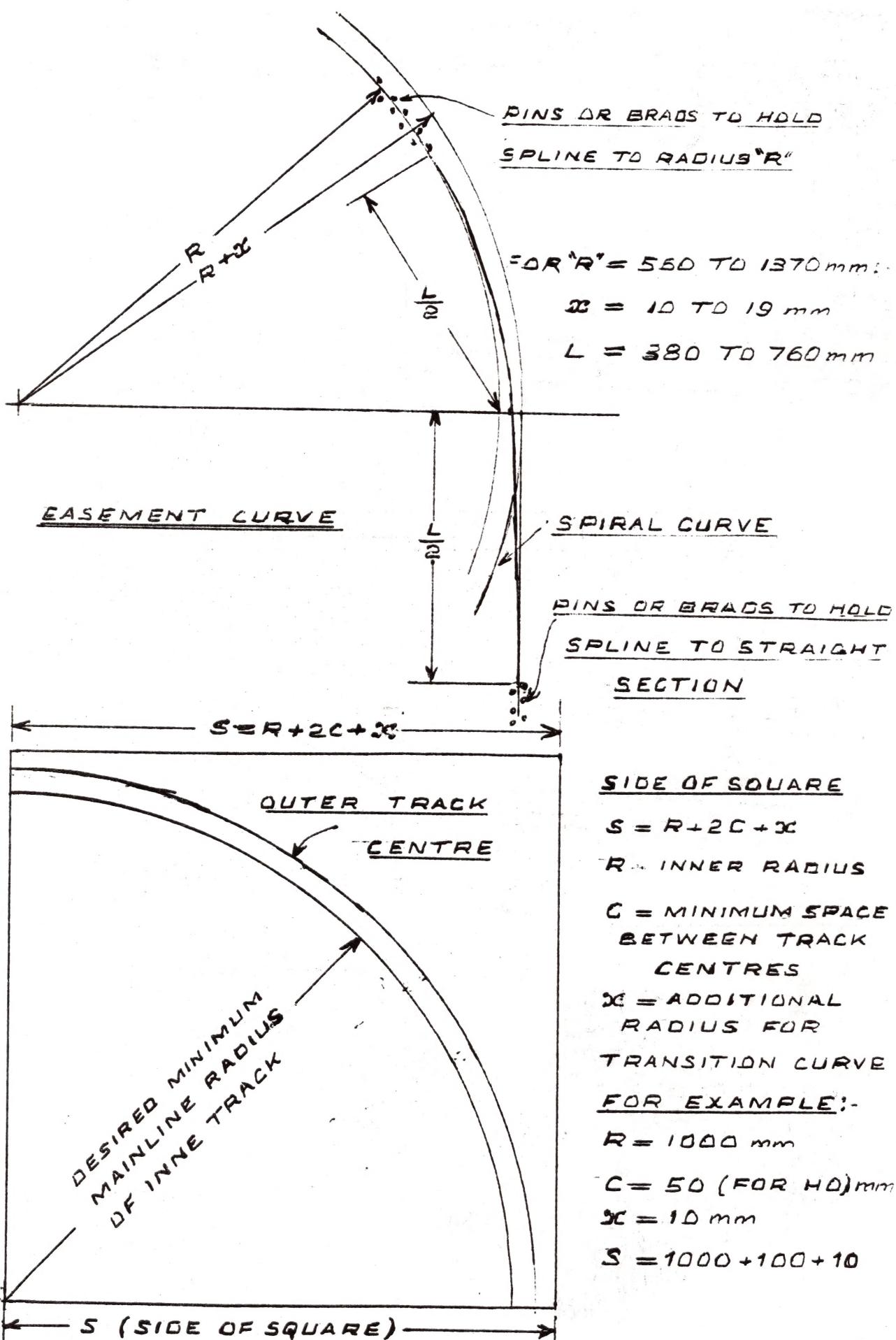
Metal strap = 1 mm x 50 mm by 18 swg 2 per module

(To butt modules and to fix to trestles)

Screws = coarse thread for hand or power tool application

Glue = PVA, aquadhere, Elmers, Bondcrete, etc.





against the first and, again, are glued, stapled and/or weighted while the glue dries. If patience will permit, an overnight drying period is more suitable.

Remove the weights and staples, rasp the cork road bed flat and bevel the sides at approximately 45 degrees. Save the cork granules for track ballast and/or scenery effects. Use the centre line of the cork road bed as the centre line of the track, and ensure that the AMRA standard clearances between adjoining track centres are maintained, eg 50 mm minimum for HO scale (see page 17 of the standards).

The application of scenery details is considered to be an art form and requires practice and skill. Whichever system or combination of systems is adopted, the Guide of Model Railways will be useful and is well worth re-reading. Over a period of many years the saw dust and plaster method has proven to be a most simple method for basic scenery application and also provides a good surface for additional layers.

As a footnote, there are many forms for baseboards. For example, a door will provide a base for N gauge; a sheet of 1800 by 1200 mm particle board, when suitably braced, will provide a small base for HO gauge; a certain modelling identity has used 6 and even 4 mm plywood sheets framed with 50 x 25 mm timber and with diagonal bracing instead of cross bracing for a large number of portable layouts. Jack Parker produced a lightweight all plywood module.

Other examples are the use of expanded foam and corrugated cardboard for the track base; and there may be others that have been forgotten. No doubt 'loxangle' metal sections could be adapted to L-Girder construction, and there is that example of costly aluminium section modules and trestles at Rockdale...

Finally, there is that story of the very, very critical modeller noted for his disparaging remarks about layouts. He was taken by surprise one evening by club members while operating his own layout - a circle of set track with several straights on a flat board on the floor! The rail for his HO layout was code 125 with a fibre base - a somewhat dated product by Wrenn, which may have preceded Triang Series 3 and 4. But no matter what, do try for that sense of achievement and enjoy the wonderful therapy of 'happy modelling'.

Spot the Makers

Can you find the names of eight toy train manufacturers hidden in the passage below?

"At last" said Eric, "I have discovered where that guy Smith lives." "That's great news. I bet Alan and Bob would pay a small fortune for that kind of information" remarked Paul. "Well said" replied Tony. "I left Bob in good hands, but this will be an additional bonus for him." "I would go and tell them myself if I could remember where I left my bike" said Paul. "It was down near the small barn, old boy, the last time I saw it" commented Tony. "Don't race and fall" Eric shouted. At that moment, Linda walked into the room, looking very slim and attractive. "Hello lads" she laughed. "You all look very pleased with yourselves."

Answers on Page 71

Spray Can Touch-up

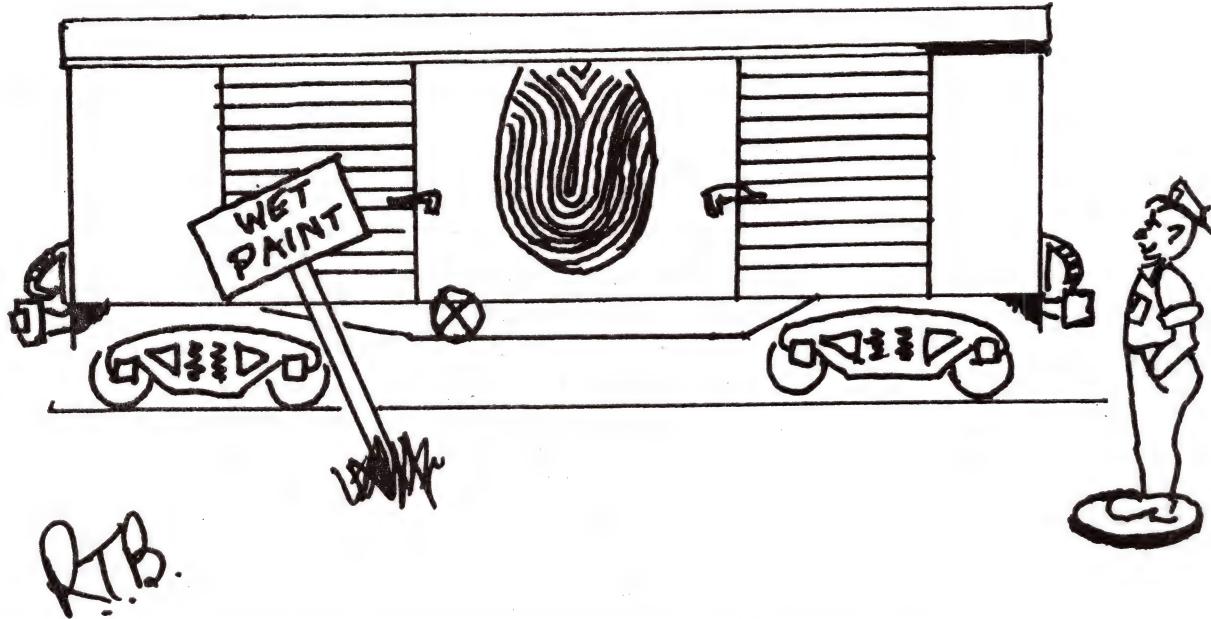
With any spray painting job, it is sometimes necessary to touch-up the paint on a model to repair minor overspray or peeled spots due to masking tape etc. When the model has been painted with a spray can, it is still possible to perform this touch-up without remasking and respainting.

Form a piece of aluminium foil into a cup shape and carefully spray some paint from close range directly onto the foil in the bottom of the cup. It will only take a couple of seconds to form a small puddle of paint which can then be brushed onto the model. The viscosity may be fairly low initially, but it will thicken gradually as the solvent evaporates.

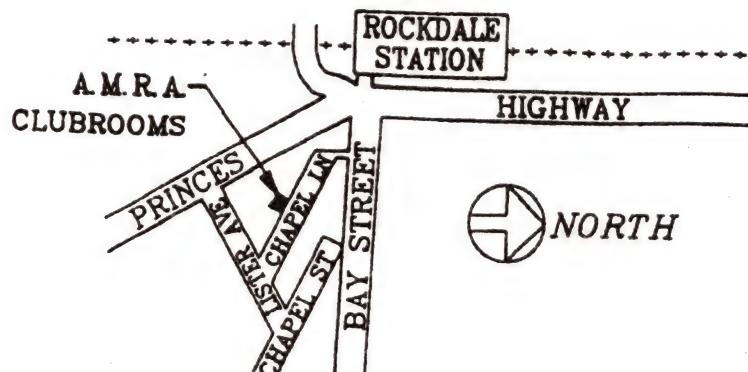
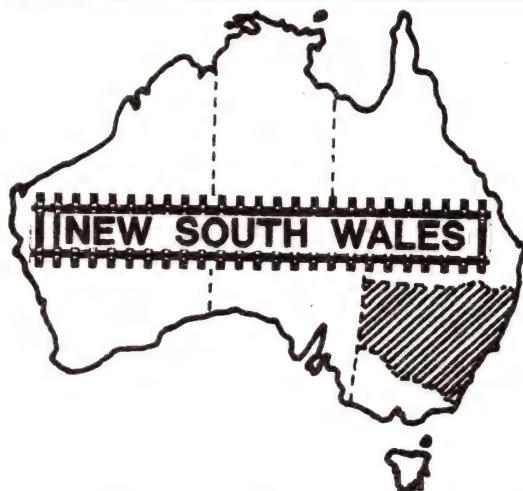
For large repair jobs, the puddle can be freshened up if necessary with another spray. Obviously, the workbench and models should be protected with newspaper in case any spray escapes from the foil cup.

A. McKenna.

"NEVER FAILS!
ALWAYS SOMEBODY
TOUCHES!"



STATE NEWS



Branch Diary

SATURDAY 6/3/93 - LAYOUT OPERATION (U.K.): 29 members and 8 guests attended this year's first outing of perhaps the most popular theme running day (?), with Bruce Thompson at the helm. -GP.

FRIDAY 12/3/93 - MODELLING COMPETITION & MODELLING VIDEO: Eddie Garde & Neil Cram of "Railscale" ventured down from the (Blue) Mountains to present their video and show off samples of their latest work on passenger cars, detailed information on which is on the "Shop" notice board. Thank you to Eddie & Neil. The first modelling competition for the year saw several items ranging from RTR-bashed passenger stock to an operating mine head. Thank you to Ross Moar and Barry Green for stepping "into the fire" and judging the competition. The next competition to be held on Saturday September 18 will have judges arranged & rules clearly laid out. 27 members and 2 guests attended with Ross Moar as Maitre d'. -GP.

FRIDAY 26/3/93 - HO PASSENGER CAR CLINIC #1: Communications having been sorted out, 25 members & 3 guests this evening had the pleasure of the real Laurie Lumsden (of "L&C Kits") presenting the clinic, the first in a series. The clinic covered the broad outline of what is to come, with discussion on the making and use of jigs to enable us to: square up the ends of plastic mouldings of car sides; square and straighten the tops and bottoms of car sides; drill for fitting queen posts; form and solder truss rods; and assemble cast metal bogie kits. Laurie also gave hints on how to make vertical cuts in car sides virtually invisible. Interested in proceeding? Laurie's next clinic will be on Friday 28th May. If you are coming, you will need to bring: Artline marking pen, 0.2 or 0.4mm tip; Steel straightedge; Small square; Assorted files; Razor saw - MUST be SHARP; Snap-off blade knife, or new sharp single-sided razor blades; MEK or similar solvent adhesive for styrene; A piece of glass, 6mm thick or equivalent, to give a flat assembly surface; A cutting board or similar, to protect the tables; and Some cash to purchase some of Laurie's car sides, bogies & other goodies. Guests interested in modelling are welcome to attend. See you there. -PK.

SATURDAY 3/4/93 - LEARN THE LAYOUTS CLINIC: 33 members attended this clinic (chaired by Barry Green), which, although slow to start, picked up before too long and provided useful training on all three fixed layouts. One or two even gained something from the HO "ovals"! The N scale layout had a leaflet describing its operation, which features elsewhere these columns. There appears to be sufficient support to repeat this topic later in the year. Thank you to all who acted as "instructors". -GP.

SUNDAY 4/4/93 - VISIT TO RICHMOND VALE RAILWAY & MINING MUSEUM: At the risk of repeating ourselves, another "top day" was missed out on by all but 14 people who were kindly chauffeured by Alan Tonks to Pelaw Main in the Hunter Valley, for a look at the last operating steam railway (closed in 1987!). The sheer scale of the operation, completely private, in its heyday is what impressed, and, perhaps unlike the museum further north, this restoration of the complete railway (Weston-Richmond Main-Hexham) is on target and making steady progress. If a "nice long steam train ride" appeals, keep your eyes on the Richmond Vale Co-op in the future, as there is great potential there. Alan Tonks' report should appear this (or next?) issue. Once again, thank you to Alan (and COM) from all who travelled - certainly "this little black duck" (a term he is apt to use) would never have made it in between night shifts if not for a minibus. (zzzzzz....where are we??....zzzzzz) -GP.

FRIDAY 9/4/93 - LAYOUT OPERATION: 25 members and 3 guests showed that even on Good Friday there are those of us who are keen to be at Rockdale! Hope all had a happy and safe Easter. -GP.

SATURDAY 17/4/93 - LAYOUT OPERATION (AMERICAS): The first Americas session for the year saw 33 members and 4 guests in attendance, with some interesting tabling on the HO layout in particular - we're not sure whether the Union Pacific loco was on tour in Europe, or the rolling stock was on tour in the States! The freelance military train also caused interest, even more so after its bogies were loosened up! Welcome to the NSW Branch Andrew. -GP.

ATTENDANCES - 1993 TO DATE:
Total: 507 members Average: 29.8

members
202 guests 11.9 guests

NSW N Scale Report

No group can achieve its goals without drawing upon outside expertise and the Mundee-niters are no exception. Back in January last year Phillip Lee gave our Planning Department a hand by advising which signals would be necessary, so that they could be provided for in the initial electronics of the layout. The resulting sixty page document is now proving vital. For example, the Engineering Department will have to cope with the installation of no less than 49 coloured light signals! This sounds awful, but coloured lights involve no moving parts. Of these, 28 will be mainline signals and the other 21 will be the smaller, shunting type. Already both series are under mass production by our Soldering Department. The shields of the signals will be old Gunnadoo Models castings. (Remember Gunnadoo? Some fly-by-night operation.) (I wonder what they were Gunnadoo?...Managing Editor)

The branchline will provide more of a challenge, because it will retain the old mechanical signals, all perfectly prototypical of course - but of the 20 signals required, most will not be operational semaphores. We are lucky that the branchline period will fairly narrow, being the 1960s and early 1970s. This will allow replacement of all working distants with landmarks, reducing the complexity of installation and the continuing pain of maintenance. Ten different signals are required on Coopers Crossing alone!

James McInerney has kindly advised us of the correct arrangement and details of the branchline stations, particularly of Coopers Crossing. This area is taking shape rapidly, with platforms, loading banks and structures underway. The manned A3 station building is being brought to life in styrene by the talented hand of Michael Chellew, who uses the spare moments behind the counter of Trains'N'Things in Sutherland to practice what he preaches. The finished model will not only set a standard for the rest of the layout but should also inspire members in other scales.

We are concentrating on the Coopers Crossing area so that at least part of the layout can demonstrate the quality and

visual appeal of which N scale is capable. There will be few buildings, the bridge is already in place, and the surrounding landscape is sparsely vegetated. In the background, creeping over the rugged hillsides, will be a few pines from a mostly painted plantation, a feature of the colder climes towards the Australian Alps. Hopefully, the impression will be that of a small turn-of-the-century passing station nestling in a bare grey landscape, with the pale green, windswept tones of the vegetation.

Following is an edited version of the principles of operation of the layout, which should be understood by those wishing to run their stock on it. Next issue of Journal will hopefully feature some photos of the layout.

Glenn Watson
NSW N Scale Reporter

N Scale Layout Notes

Principles of Operation

There are five control panels on this layout, each having a mimic diagram of the track layout for its area of control. The green tracks are the mainlines, red are the sidings and yellow are dead-end sidings. Each panel is equipped with toggle point switches, point-indicating LEDs and rotary/toggle block switches.

The point switches are of the centre-off type and are spring biased to that position. To throw a point press the toggle towards the desired direction of travel, the point should throw and the LEDs on the panel should indicate the new position of the point. At crossovers, pushing the toggle up will set the crossover arrangement, while pressing the toggle down will set the straight through arrangement.

Each panel has a rotary switch and a miniature toggle switch for each block. The rotary switches are for selecting which controller is powering the blocks; the miniature toggle switch is used to turn the blocks off or on. There are five remote controller positions and one local controller position for most blocks (the exception being at Bennett Station). At present there is only one controller that will work right the way around the layout, this is assigned the green position on the rotary switches and is located under the layout near the stairs to the mezzanine level.

Each main panel (Bowen, Wallamudra & Watson's Flat) has a local controller; they are black plastic boxes with a rotary knob for speed control, a switch for direction control and three LEDs indicating: power in, Green; power out, Yellow; short circuit, Red.

Eventually all five remote controllers will be available hopefully in a cordless format; also the computer under the layout will control signals around the layout with further advances as time goes on. Work on this layout is carried out on Monday nights from 7:30pm to about 10:00pm most weeks. If you feel that your talents may be of use to us please let us know, especially in the areas of printed circuit board manufacture, scenery work and scratchbuilding buildings.

D.R. Bennett
Chair, N Scale Subcommittee

O Scale Layout Notes

and a Thank You

The Branch is fortunate that long time member Bruce Lovett has found time since retiring to have a sort out amongst some of his treasures and bring them along to the Club - amongst these are packets of small alligator clips, in the Shop at \$1 per packet of 10; also some boxes of various odds & ends for disposal to members with a (min.) \$2 donation by the member for whatever they take; also a donation of magazines for the Library, some bound or boxed. In one of the boxes will be found some history of our early exhibitions, including the layout of our first exhibition which was held at the Burwood Police Club; also one of the first held at the Sydney Town Hall. Incidentally Bruce was Branch President at this time and organised the Burwood exhibition in 1962; as this was a success, at the next COM meeting Bruce said, "I think we should go for the Sydney Town Hall next year." When the committee recovered, it was further discussed and became fact and history. There is also a collection of the early dodgers - boy oh boy, adults 3 shillings (30c), pensioners & children 1 shilling (10c)! It is a good thing to have some records of our early history, so thank you Bruce. Being an old O gauger he has undertaken to do some scenic work on the side walls around the O gauge layout - I say "O gauge" because there is equipment in more than one scale running on the 32mm track. So far the walls have been painted sky blue; clouds will be next, then some hills, which should look effective when viewed across the layout. How many have noticed that the footbridge on the junction station [Trainsville] has been lowered in height and repainted?

Norm Read
Chair, O Scale Subcommittee

AMRA NSW Clubwear

At the time of writing, over 80 orders have been received for shirts and jackets for NSW members. Hopefully you would have seen them prominently at the clubrooms! At this time, the next order deadline is open, due basically to just about all the Rockdale regulars having ordered garments already! All new Branch members should now receive an order form in their membership kit but, new member or "old", it is never too late to place an order - order forms are always available from the clubroom - call, call in or write for one. Please note though that, from August to January, jackets may be ordered but will not be available for delivery from the manufacturer. Next on the agenda (if sufficient demand exists) is name badges, and perhaps in the medium term screen printed T-shirts.

Club Programme Notes

Expressions Of Interest Required

The Club Programme presented this issue incorporates a few suggestions, viz:

- Saturday afternoon audio/visual meets;
- More trips, both metropolitan & out of town; and
- A second modelling competition, prior to the Exhibition.

It is never too late for other programme suggestions, so if you have them, PLEASE jot them down and send them to the Committee.

EXPRESSIONS OF INTEREST are requested for the following two venues which are being considered. Numbers are being sought before tabling a date for these excursions, as each venue will be limited to 10 persons at a time:

- Qantas Jet Base, Sydney Airport To be led by Barry Green Itinerary to be finalised, but should include simulators - Civil Aviation Authority, Sydney Airport To be led by Glenn Percival Itinerary: Air Traffic Control & Technical Areas

If you would like to join either or both of these trips, get in touch with the Publicity Officer. By the time you read this, a clipboard (as used for minibus trips) should be at the Club.

OFFERS OF HELP ARE BEING SOUGHT for a proposed "AMRA NSW Video Magazine", the idea being to capture on video highlights of each meeting, to form a record of happenings at Rockdale able to be borrowed from the Branch Library. This project is simply beyond the resources of the Branch's "resident videographer", however, with the sheer number of other members with camcorders it should be possible to collect a few minutes' worth of each meeting. If you are able to help, please contact the Publicity Officer. With enough volunteers, perhaps it could be rostered along with the Duty Officers....?

Duty Officer Crew

Since the post-Exhibition 1992 Open Day the reintroduction of the Duty Officer concept has met with success. Their duties include: opening & closing the clubroom; greeting guests at the door and seeing that they are attended to; the general conduct of meetings; and making sure a report for "Journal" is made (where applicable). A roster of willing volunteers is assembled by the Publicity Officer, with a big thank you (and welcome!) to the following hardy souls having put their hands up thus far: David Bennett, Danny Elliott, Damien George, Barry Green, Phil Kelly, Philip Lee, Ross Moar, Glenn Percival, Graham Saint, Bruce Thompson, Alan Tonks, and Brian Tyson. As you can see, Duty Officers need NOT be Committeepersons, but are subject to Branch COM acceptance. If you think you would like to actively help the Branch in this way, please contact the Publicity Officer. With a couple more people on the Duty Officer roster, it will only be necessary to work once every few months.

AMRA NSW Library

The Branch Library of books, magazines, videotapes and other reference works is available to all members, for a nominal charge of \$2.00 for twelve months. An ongoing stock take is being conducted, so access to certain works or groups of works may be restricted at the discretion of the Librarians. For more information contact the library staff, one or more of whom are usually present each meeting day.

THOMAS COOK TRAVEL, WORLD RAIL DEPARTMENT, HAS PROVIDED THE BRANCH WITH DETAILS OF THEIR \$6300 AUGUST 1993 RAILFAN TOUR OF BRITAIN. CONTACT THE PUBLICITY OFFICER FOR FURTHER DETAILS.

Programme May to October 1993

MAY

Sat 22nd "Daylighters" Audio/Visual: Members' contributions
 Fri 28th Clinic: HO passenger cars #2 Laurie Lumsden, "L&C Kits"

JUNE

Sat 5th Members' Auction (please book lots in by 2pm)
 Fri 11th Clinic: HO passenger cars #3 Laurie Lumsden, "L&C Kits"
 Sat 19th Layout Operation - HO & N scales only (General)
 ** OPEN DAY ** 10:am to 5:30pm
 Fri 25th Slides: theme to be advised - Col Gilbertson
 Sun 27th Visit to Illawarra Live Steamers, Nth Wollongong
 Contact club for arrangements.

JULY

Sat 3rd Layout Operation (Work Then Run) 10:am
 Priority given to workers.
 Fri 9th Clinic: HO passenger cars #4 Laurie Lumsden, "L&C Kits"
 Sat 17th Layout Operation (Australian)
 Fri 23rd Clinic: Club Layouts Details to be advised.

AUGUST

Sun 1st Layout Operation (General) 12:00pm
 Sat 7th Locomotive Drawbar Pull Competition
 Fri 13th Movies - Fred Stell
 Sun 15th Visit to Western Districts Live Steamers, Fairfield Showground
 Sat 21st Layout Operation (General)
 Annual Dinner 6:30pm Booked Patrons Only
 Fri 27th Layout Operation (U.K.)

SEPTEMBER

Sat 4th Layout Operation (Work Then Run) 10:am
 Priority given to workers.
 Fri 10th Layout Operation (Americas)
 Sat 18th Modelling Competition plus Daylighters
 Audio/Visual: "By Request"
 Fri 24th Exhibition Preparation
 Sat 25th Exhibition Preparation (if necessary)

OCTOBER

Fri 1st Exhibition Setup at Liverpool 10:am
 Sat 2nd - Mon 4th ***** THE 31ST SYDNEY MODEL RAILWAY EXHIBITION *****
 Fri 8th Audio/Visual: "Members' coverage of Exhibitions"
 Sat 9th Visit to Bankstown Live Steam Society, Ruse
 Bankstown
 Sat 16th ** OPEN DAY ** 10:am to 5:30pm
 Fri 22nd Clinic: HO passenger cars #5 Laurie Lumsden, "L&C Kits"
 Sun 31st Visit to Sydney Society of Model Engineers.
 Model Park Open Day, Luddenham

NOTES

WORK DAYS: Wednesdays, from late morning. Most Monday nights, 7:30-10:00pm.

Continuing Clinics are held on Layout Operation days, covering such topics as model making, kit bashing, detailing, and throttles.

On Layout Operation days, priority is given to that meeting's theme, or as indicated.

Guests are welcome at meetings unless otherwise indicated.

Meeting times, unless otherwise indicated, are: 1st & 3rd Saturdays 2:00-5:30pm 2nd & 4th Fridays 7:30-11:00pm.

Clubrooms are situated at north end of Chapel Lane, Rockdale. Phone: (02) 567-1899.

This programme is subject to change without notice. Updated programmes are available from the Clubrooms - phone or write.

All correspondence on N.S.W. Branch matters should be addressed in the first instance to:

The Secretary, AMRA NSW, PO Box 194, ROCKDALE NSW 2216.



EXCITING NEW TITLES FOR RAILWAY MODELLERS FROM MASTERCRAFT VIDEOS

"MOVING MOUNTAINS"
 Three spectacular classic layouts of European networks. "Europ", "Charmes" (N gauge), "Jura-Simplon". **\$39.95** (50 minutes)

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 Contrasting techniques. Landscapes of gorges, valleys and hills in 'Lynbridge' and 'Chee Tor'. **\$39.95** (50 minutes)

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 Step by step guide to building a working layout. **\$39.95** (60 minutes)

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The AMRA NSW Shop

CLUB SHIRTS

Adult sizes 14-30 \$26.00 ea.

Postage (if desired) \$2.00 ea.

Personalised embroidered BONDS "The Penguin Shirt" polyester/cotton pocketed shirts. 'Natural' (beige) in colour, with AMRA logo on non-pocket side & your name above pocket.

CLUB JACKETS

Adult sizes 14-22 \$35.00 ea. Child sizes 6-16 \$28.00 ea.

Postage (if desired) \$5.00 ea.

BONDS premium jersey fleece press stud jacket, embroidered and personalised as above. "Timber" (dark brown) in colour. Delays will be experienced unless ordered February-July.

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A.M.R.A. in '92	\$18.00 ea.
AMRA '91	\$15.00 ea.
BOTH together	\$24.00 ea.
A.M.R.A. in '92 APPENDED to your AMRA '91	\$ 8.00 ea.
\Postage (if desired)	\$ P.O.A.

CALL IN, CALL OR WRITE FOR ORDER FORMS FOR THE ABOVE!

GAUGES to AMRA Standards

LIMITED STOCKS!

HO GAUGE Wheel/Track Code 100
\$ 3.40 ea.

Code 70 \$ 3.40 ea.

Check gauge \$ 6.80 ea.

N GAUGE Wheel gauge \$ 3.40 ea.

Track gauge Code 80 \$ 3.40 ea.

Code 55 \$ 3.40 ea.

Selleys "Aquadhere" one litre bottle
\$ 5.00 ea.

Aluminium diecast G-clamps \$ 1.50 ea.
two inch (51mm), with plastic foot & handle
C&K brand quality switches

#7201 DPDT \$ 3.50 ea.

#7203 DPDT centre-off \$ 3.50 ea.

#7105 DPDT centre-off momentary action
\$ 4.00 ea.

(for your hand throttles!)

Light Emitting Diodes (LEDs) 1.5mm red
\$ 0.75 ea.

Small alligator clips packet of 10
\$ 1.00 pkt

AMRA car window decals \$ 0.50 ea.
affix to inside of back window

CORK ballast strips

1/8" (for HO gauge) 1m x 4cm \$ 0.60 ea.

1/16" (for N gauge) 1m x 2cm \$ 0.20 ea.

ODDS & ENDS Donated by Bruce Lovett
HURRY! \$ 2.00 min.

Minimum donation to Branch requested
SAMPLES ONLY:

Genuine Pelton Australian scale coal 150g
bag \$ 8.00 ea.

Prototype 160mm max. In O, HO & N
scale.

Layout Subcommittees

Subcommittees have been formed to give broader direction to the task of developing the layouts, and to "spread the load". The Subcommittees are not expected to be the only ones to actually work on the layouts; and any input (e.g. concepts for an area) from other members is most welcome. The Subcommittees are as listed below, and their brief (paraphrased) follows:

N SCALE David Bennett (Chair)

Glenn Watson

Neil Watson

John Lischeld

HO SCALE

Ralph Wambeek (Chair)

Arthur Harris

Ed Hogan

Bob Wardrop

Fred Stell ("CTC")

HAWKESBURY RIVER/ KNAPSACK GULLY

Alan Tonks (Chair)

Glenn Percival

NEW HO EXHIBITION LAYOUT

Phil Kelly (Chair)

Jack Parker

Alan Tonks

Glenn Percival

Bob Wardrop

O SCALE

Norm Read (Chair)

Brian Tyson

Graham Larmour

Bert Hetherington

LAYOUT SUBCOMMITTEE

BRIEF (paraphrased):

a) Under guidance of the Chairperson, be responsible for the development, operation & maintenance of their respective layout(s). b) Each Subcommittee is allowed a set maximum expenditure per month, non-cumulative, without prior approval. All expenditure must be substantiated with receipts, etc. Expenditure beyond this amount MUST be submitted to and approved by COM prior to action. c) ANY MEMBER IS WELCOME to suggest improvements, modifications & developments to a layout. The member should present same to the relevant Subcommittee for consideration. d) Work on a layout is to be commenced only after approval from the relevant Subcommittee. e) Subcommittees may ask for demonstrations to be presented for approval to ensure that work is carried out in an acceptable manner & to an approved standard. f) Subcommittees should present to the Publicity Officer regular reports on layout matters for inclusion in "Journal".

Members' Discounts

The establishments listed below have indicated they will favour A.M.R.A. members as detailed. To obtain the consideration listed, members MUST show a CURRENT membership card (establishments know what to look for), and be at least a little discreet. In the words of one shop, "When we have offered extra discounts we have been embarrassed with the situation of such a member announcing that he should get a discount on \$2 worth of track pins. Other customers then cannot comprehend why such a clown gets 10% when the twenty or thirty dollar purchase they are making doesn't seem to qualify."

HOBBYCO, CITY will offer 10% off across the store, except markdowns and specials.

PUNCHBOWL HOBBY CENTRE, BANKSTOWN will offer 10% off, except consigned goods.

SYDNEY HOBBIES, DRUMMOYNE will offer 10% off models only, cash or cheque customers. A larger discount may be available for bulk (Club) orders.

THE MODEL RAILWAY CENTRE, GYMEA will offer 10% off, except on consigned goods.

TOYMAN IMPORTS/YENNORA HOBBIES, YENNORA will offer further savings on their regular prices wherever possible.

TRAIN N THINGS, SUTHERLAND now offer 10% off, except on books, magazines and videos.

VAGGS HOBBIES, MIRANDA will give 10% off.

CRONULLA STEEL FABRICATORS, 6/141 Taren Point Rd CARINGBAH, will

give favourable consideration to members. This firm was involved in shoring up the frame that the "Hawkesbury" layout travelled in.

More establishments will appear as confirmed, and this list also appears on the "A.M.R.A. N.S.W. Shop" noticeboard in the foyer.

Answers to 'SPOT THE MAKERS'

"At last" said Eric, "I have discovered where that guy Smith LIVES." "That's great news. I BET ALAN and Bob would PAY A small fortune for that kind of information" remarked Paul. "WELL Said" replied Tony. "I left BoB IN Good hands, but this will be an additional bonus for him." "I would go and tell them myself if I could remember where I left my bike" said Paul. "It was down near the small bARN OLD boy, the last time I saw it" commented Tony. "Don't race and FALL" ERIC shouted. At that moment, Linda walked into the room, looking very SLIM And attractive, "Hello lads" she laughed. "You all look very pleased with yourselves."

Heard at the Exhibition!

My wife said she would leave me if I bought any more trains.
Boy how I miss her!!

For Sale

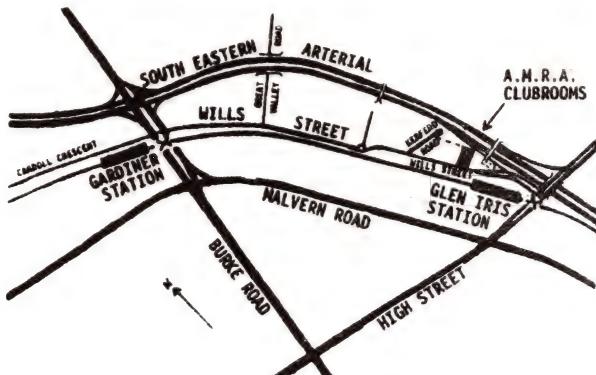
Huge HO Layout

32 feet by 50 feet
6 foot radius minimum
curves

150 Shinohara points

Extra bonus! Comes
with modern two storey
house attached in
suburban Brisbane
(Kalinga) 9 km from
city centre.

For more information
contact Harry Parks
Phone 07 357 6373



General Notes

Daylighters Sessions

These are held on the first, third and fifth Mondays of each month, as well as the second and fourth Thursdays of each month. The sessions consist of a couple of hours working bee in the morning, then lunch followed by operating/running on the layout. During 1992, 27 Monday sessions, together with 2 Thursday sessions were held with the following members attending: A Johnson (29 sessions), G Turner (28), R Marsden (27), A Burton/C Hill/J Treseder (26), Les Johnstone/A McKenna (25), A Woods/J Gardner(22), T Dunlop/L Bugeja (19), R Dunn (15), N Hambley/J Harry (11), J Kerr/R Baker (10), N Riches (9), K Wilhelms (8), B Southwell (7), D Marsden(6), R Pearson (4), Leo Johnstone/W Fock (3), I McKenna/P England/S Pattison/H Croft/ R Polistena/R Thomas/J Pearson (2), J McDonald/D Norman/B Wedge/Jack Kerr/ B Edwards/F Croft/J McClure/S Dobney/N Dobney/B Wilhelms/K Dillion/S Lambert/ G Serpel/D Morrison (1).

SLSV Open Day

The Seam Locomotive Society of Victoria at Rowans Road, Moorabbin, have arranged an 'Open Day' on Saturday 22 May 1993 from 10.30 am to 5 pm. BBQ facilities, BYO lunch and everything, tea & coffee available. If you are bringing a locomotive to run, a current Boiler Certificate will be required and inspected before running may commence. Name tags and identification would be appreciated.

Auction Day

The annual auction will be held on Saturday 19 June 1993 (changed from Saturday 12). Lots for auction to be in by 1 pm. Lots will be open for inspection etc from 1.15 pm with the auction to start as soon as possible after 1.30 pm. Forms will be available at the June meeting. Commission as follows: Sold lots: 10% of "knock down" value with a minimum of 50 cents per lot. Unsold lots: 10% of "final bid" value with a minimum of 50 cents and a maximum of \$2 per lot. All lots must have a reserve price(s) noted on the form.

Latrobe Valley Exhibition

Further to item on Page 42 of March-April 1993 Journal, we will be loading the truck at the Clubrooms commencing 10.30 am (not midday as previously stated) and require to be at Morwell to unload by 3.30 pm. Latrobe Valley Model Railway Asso-

ciation (Organisers of exhibition) will provide accommodation overnight (if required), Saturday lunch, Sunday lunch & evening BBQ, Monday lunch and evening sandwiches for participants.

Camberwell Exhibition

The Treasurer reported that attendance for the 1993 Exhibition was down on the previous year, but a change in admission prices for Pensioners lifted takings above last year. Some advertising on TV Channels 9 & 10 was obtained, but no TV News items occurred. A nett profit of approx \$16 000 was made; this being before having to pay 38 cents in the dollar tax of the nett profit. Overall, the Exhibition was successful with sufficient profit made (even after tax) to keep the Club and rooms functioning for the next 12 months.

Taxation Matters

The Branch has had the appeal to the Tax Office for tax exemption refused.

European Timetable Sessions

These sessions will be held on the last Friday evening of each month, except December. Note that some months have 4 Friday evenings, whilst others have 5 such. Commencing time will be 7.30 pm.

Timetable Session Alteration

The timetable session on Thursday 22 July 1993, which was to be European, has been changed to USA pre-1960 prototype.

March Meeting

Models for Display

R Thomas - 4 wheel etched underframe for GY type wagons by N scale collective.

S Westerman - Mini vacuum cleaner to attach to home vacuum

Competitions

Models

Kitbashed Athearn SD4D-2B DE - Keith De Souza - 92 pts

Kit - Murphy's Grain Store - Ian McKenna - 85 pts

Photos

Print - Railway Fuelling Point at Seymour - Stuart Westerman - 80 pts

Slide - No entries

As no syllabus item was held, the rest of the evening was spent with running on the layout.

April Meeting

Models for Display

B Smith - Model Engineer & Electrician Magazine issues covering 1908, 1909 & 1911

P England - Micro vacuum attachment kit
S Westerman - Tortoise switch machine to work quadrant signal

G Nitz - 2 off Pennsylvania RR Cabin Cars (caboose) from Bowser with Cal Scale Antennas - one car in Tuscan red livery and the other in red livery.

G Nitz - Narrow Gauge European Fleishman 'Magic Train' - Oe Scale (USA ON 2 1/2) Open Wagon

Competitions

Models - Kit Lloydhurst Public Hall - I McKenna - 90 pts

Photos - Slide B69 at Geelong - I McKenna - 96 pts Print B at Sunbury - L Bugeja - 95 pts

The syllabus item was a 'show & tell' session with the following members participating:

1. Ron Welsh - Hornby Dublo 3 rail, Goods Tank Loco Set.

2. John Gardner - Canberra mixed train 1960/70 era featuring:

DJH models Garrett AD 60 (DC version) Steam Loco (constructed/assembled by Trevor Reeves).

FO Coach by Casula Hobbies.

VAC Spirit of Progress sleeping car by Powerline.

Freight rolling stock by Lima, Powerline, BGM and handbuilt.

3. Peter England - 1960 era DMU set by Lima (Swindon 3 unit Cross Country Set) modified wheel sets and internals.

4. John Davidson - Bachman Norris loco 'Philadelphia', 1840 era, Germany-type (similar to 'Rocket') with one driven wheel plus 3 Bachman Coaches from 'The Collectors Series'.

5. Rex Little - NSW Passenger Set includes: Repainted 'Candy' livery 44 class single motor DE (No 4480) Trax & Powerline Carriages in normal livery and 'Candy' livery. Loco fitted with slow running mechanism from Casula Hobbies.

6. Blair Westerman - Double headed LNER train. Bachman B2 (tender) steam loco & B (tank) steam loco from 1936 era and Hornby coaches.

7. Neil Hambley - USA Great Northern train of early 1950 era. Loco - GN J class 2-6-2 Prairie by Samson/Oriental. Coaches (3 off local set) - GN by Walthers with Central Hobby bogies and painted by Neil.

8. Stuart Westerman - Intercity 125 set in Executive livery, modified pick up using leading coach as well as loco. The set ran on layout 'Ashford & Littlemoor' at Camberwell Exhibition and clock 125 km per hr (scale speed).

9. Stuart Westerman - Two car Super Sprinter set by Lima (modern era).

10. Alan McKenna - United Models - Climax loco & generic 3 truck with Kadee Line disconnected log buggies (wagons) and KD Van (wagons operated in pairs).

11. Peter England - City of Truro 4-4-0 Scratchbuilt loco with Portescap mechanism, hauling standard Hornby clerestory coaches.

Thanks to all participants in giving other members a chance to see these interesting models.

Exhibition Layouts

The situation with regard to layouts that the Branch can exhibit has greatly improved in recent years. Presently the Branch has the following layouts:

1. A 6' x 4' American desert scene layout (Barney's).
2. A 8' x 4' UK (Linton) layout.
3. A 8' x 5' UK 'U' Drive layout.
4. Kyneton VR layout (29' x 15').
5. Wills Street (USA Commuter & Industrial) Layout (29' x 15').

Each of these layouts has been an improvement on the previous one and credit is due to those whose efforts have contributed to these achievements.

Programme

Addition to May

Fri 28 1930 Timetable Operation - European prototype

Addition to June

Fri 25 1930 Timetable Operation - European prototype

July 1993

Sat 3	Junior Day -10 am to 3 pm - BYO
Sun 4	Timetable Operation -1.30 to 5.30pm - USA prototype
Mon 5	Daylighters -10 am to 3 pm - BYO train
Thu 8	Daylighters -10 am to 3 pm - BYO train
Thu 8	Social Meeting - 7.30 pm - Rick Schonfelder - Slides of Canadian Trains
	Model - Standard Categories
	Photo - VR 'R' Class
Wed 14	Junior Day -10 am to 3 pm - BYO train
Mon 19	Daylighters -10 am to 3 pm - BYO train
Thu 22	Daylighters -10 am to 3 pm - BYO train
Thu 29	Timetable Operation - 7.30 to 11.30 pm - USA pre 1960
Fri 30	Timetable Operation - 7.30 to 11.30 pm - European prototype
Sat 31	Junior Day -10 am to 3 pm - BYO train

August 1993

Sun 1 Timetable Session -1.30 to 5.30 pm - Aust prototype

Mon 2	Daylighters -10 am to 3 pm - BYO train
Thu 12	Daylighters -10 am to 3 pm - BYO train
Thu 12	Social Meeting - 7.30 pm - Agenda item to be advised
	Model - Standard Categories
	Photo - A Garrett
Mon 16	Daylighters -10 am to 3 pm - BYO train

1993 Exhibition

The annual Camberwell Exhibition was held over the March long weekend. It was very successful again this year. I would like to thank the exhibitors and members who assisted in the presentation of the Exhibition at Camberwell.

One very pleasing aspect of the Exhibition was the new layouts on display. The AMRA American layout, Wills Street, was exhibited for the first time at Camberwell. It had only been on show previously at the AMRA open day at the Club Rooms. North of the Border and Rio & South West Pacific, both from Queensland, were displayed in Victoria for the first time. Another new layout, Avenel, was on show for the first time.

Also the Exhibition Guide in its new format was very well received.

The following Awards were made:

BEST PRIVATE LAYOUT

Wingham - Rodney James

BEST SOCIETY OR GROUP DISPLAY

Croydon Narrow Gauge Group

BEST COMMERCIAL DISPLAY

Precision Scale Models

BEST DISPLAY BY PUBLIC VOTE

North of the Border - Darcy Brittain

EXHIBITORS CHOICE AWARD

North of the Border - Darcy Brittain

Open Modelling Competition results were as follows:

WINNER OF AMRA CUP - BEST MODEL

Philip Dunne - VR 3ABC Car Van

BEST LOCOMOTIVE

Frank Kelly - 1880 VR S201 Locomotive

BEST ROLLING STOCK

Passenger - Philip Dunne - VR ABC Car Van

Freight - Philip Shapter - Work Gondola D & RGW 1004

BEST LINESIDE STRUCTURE

Graham Fraser - VR 4 Arm & 4 Disc

Lattice Mast Signal

FYFE THORPE AWARD - BEST AUSTRALIAN PRODUCED KIT

Geoff Trueman - VR First Class Coach 30 AE

**Bob Edwards
Exhibition Manager**

Property Manager's Report

The Vic Branch property has been improved over a period of time by members and 'The Daylighters' group, in particular.

The Daylighters have been completing maintenance and improvement projects and have continuity with the housekeeping tasks.

The current major project is known as 'The Great Wall of Glen Iris'. This is the retaining wall at the front of the property to provide an area for the continuation of the outdoor railway that is near the side

entrance. It is not all heavy work, such as lifting blue stones; there are less strenuous tasks, not the least being 'putting the kettle on', which enables members to contribute to the project.

The project has reached the halfway mark!

**Jack Treseder
Property Manager**

Special Notice

Prior to and during our successful 1993 Exhibition, there were a number of criticisms, mainly by innuendo, and completely unfounded, levelled at our Exhibition Manager, Bob Edwards. As a result, Bob resigned from the position that he has very capably filled for the past 14 years. A vote of thanks was carried by acclamation at the April Meeting.

The COM are seeking expressions of interest from members who would be prepared to take on the position of Exhibition Manager, or be a member of the Exhibition Committee for the 1994 Exhibition to be held at the Camberwell Centre over the period 11 to 14 March 1994. Letters have been sent to all Vic Branch members about this matter.

**John Harry
Secretary**

Library Notes

Nostalgia time! Remember 1988? A vintage year indeed, and *A Vintage Year for Steam* by Steve Malpass, John Dare and Ian Jenkins pays it due homage. *Aus Steam 88*, the travels of 3801 and, of course, the adventures of the Flying Scotsman are all there, with hosts of photographs, in b&w and colour, and including a chapter by Wal Stuchbery on the way it all got started, and the way it felt once the whole juggernaut began to roll. There's an introduction, too, by John King, formerly Victorian Director-General of Transport, paying tribute to the work our Victorian Branch members Wal and Di, and John Frost, put into the project. Well deserved indeed. And, as they say, lots, lots more, all in a beautifully produced landscape format. Our copy is in limp covers, but I believe there's a hard-cover version due any minute.

John Sargent who brought us the spectacular photography of the boy Carl Stratmann in volume 2 of his *Rail Scene Victoria* has now done it again, with volume 3, entitled "*Steam on the Broadgauge in the 1930's*". The emphasis this time is on the station and the road rather than the shed, but the photos are just as evocative, and the book production as fine. Thank you John, for the production and for giving a copy to the Library.

Rail Scene Victoria has also expanded into videotapes, the first venture being R Class: "*A Contemporary Legend*", videotaped, edited and mastered by Greg Naylor. Again thanks to John Sargent for the donation of a copy.

Phoenix to the World by James Murray is a company history, but one of special interest to all Australian railfans. The company is Clyde Industries and among their various products are many of Australia's locomotives. Why Clyde? The number of Glaswegians in the company, and therefore in the district, and its loca-

tion on a river (Duck River, near Sydney) meant that the name just seemed right. And the phoenix in their trademark, and in the title of the book? The answer is that the quite enormous and diversified firm that William Henry Hudson set up in 1855 (coincidentally the year a railway joined Sydney and Parramatta) collapsed at the end of the century, to be revived by a syndicate including his son, Henry.

The range of manufactures produced by the company is immense, the range of companies started or taken over is itself startling, and the book is set as much at the board table as on the factory floor. Perhaps there is just a teeny bit too much on the exploits of Sir Raymond Purves, absolute dictator of the company until his death in 1973, but company histories are like that. And no company history, of a company that produced 3801 and her sisters, could be all bad.

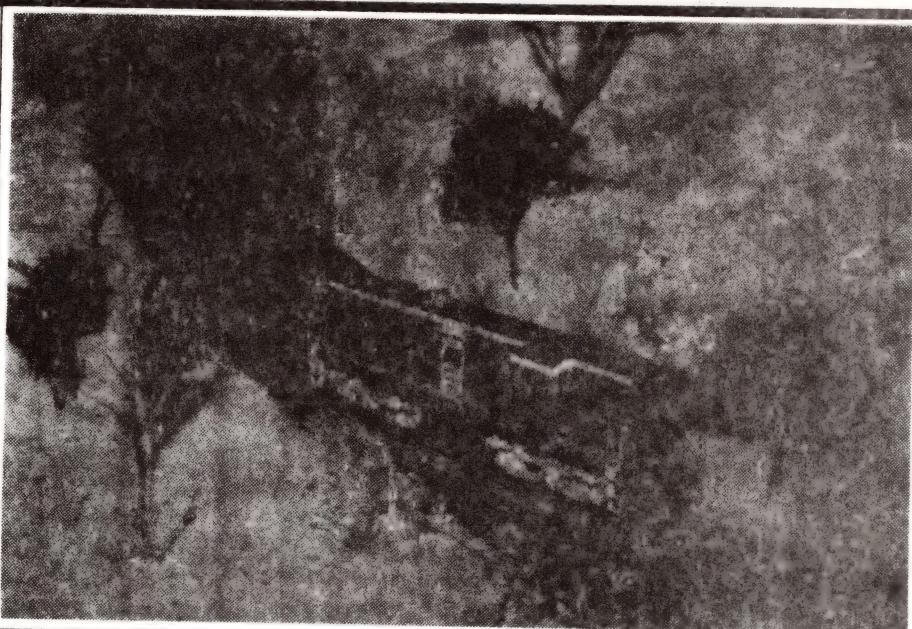
And, in contrast to the big and brassy, a nice little book of nostalgia for Liverpudlians: John W. Gahan's *Seventeen Stations to Dingle*, a history of and a memorial to Liverpool's Overhead Railway. It took the workers to the docks, it took those meeting the great liners, it took the sightseers and gave them the best of all views of the throbbing dockland. And when that great beast lay down and died, the little railway died with it.

Brian Southwell
LIBRARIAN

AVMRC Convention

The Association of Victorian Model Railway Clubs is holding an extended one-day convention on Saturday 4 September 1993, commencing at 8.30 am until 9.30 pm at Collingwood Masonic Hall.

The format will be clinics and workshops. At the Workshops, the Presenter will demonstrate his successful methods and techniques and answer questions on the subject.



Clinics will be given by a Presenter in a classroom mode with attendees having the opportunity to ask questions.

Also being tried at the convention is the opportunity for prototype modellers to assemble and discuss their prototype stock. The prototype groups are Victoria/Australian, UK/Continental and American.

There will be a hospitality area for attendees to just mingle and talk.

Proposed subjects for this convention are:

Wiring, Electronics, Bridges, Scenery, Weathering, Research and Modification.

The programme has been arranged so that attendees must choose the subjects that best suit their needs.

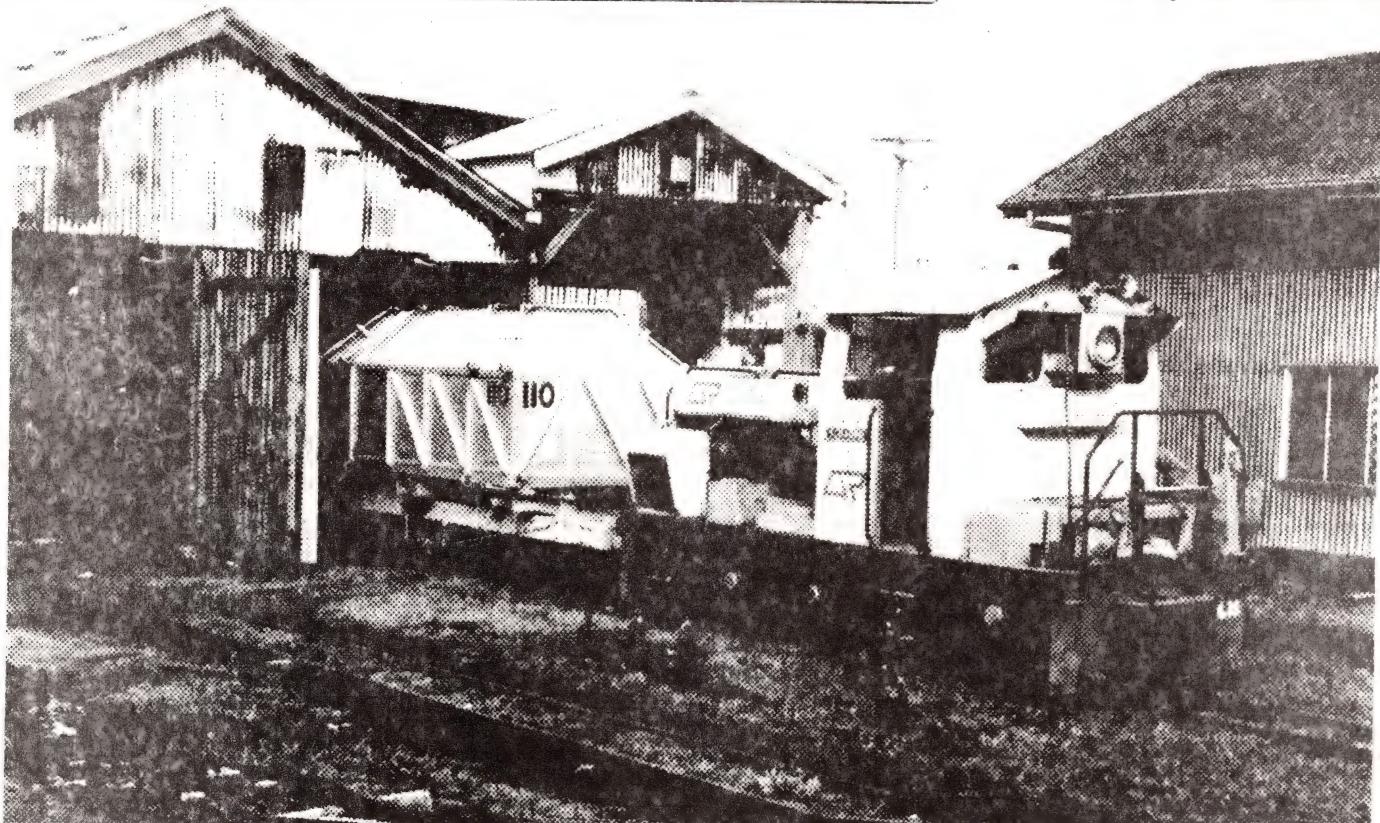
For further information, please refer to 'The Convention Registrar', 55 Creek Road, Mitcham, 3132.

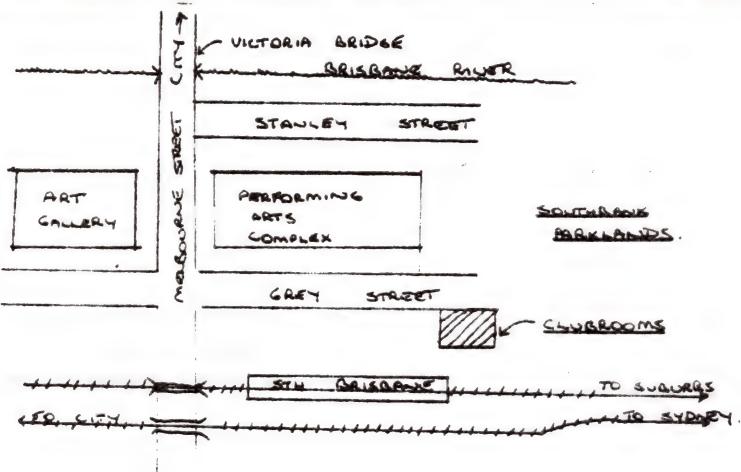
Jack Treseder
Convention Registrar

A Scene Worth Modelling - The Charm of Narrow Gauge

Years ago, the Queensland Railways had an extensive, profitable two feet gauge operation in the Innisfail area, North Queensland. Over 10 years ago, the operation was sold up and dispersed to the relevant sugar mills nearby. In this scene of Queensland Railways "Narrow Gauge", an 0-6-0 Com-Eng Loco 'Karabine' shunts a bogie wagon fitted with a Bulk Raw Sugar container into the workshops for attention. The locos were a colourful yellow, red and black livery, the bogie wagon has silver container, grey wooden underframe and black undergear. Note the workshop with its obvious add ons and corrugated iron cladding, showing various degrees of decay. The track shows no signs of sleepers under the grass and oil stained dirt. In front of the loco are scattered many spare couplers, standing up in a random display.

Photo by John Elsol 1972





From the Blast Pipe

Well the battle has been fought and won for another year. Once again the call went out from A.M.R.A. Qld to all of its members to help make the 1993 show a success and that's exactly what happened. With sponsorship from 4KQ (thanks very much) and several articles in regional newspapers we certainly received fairly wide coverage prior to the event.

As always, everybody who helps out deserves a big thank you for the time and effort put in. Whether it was ten minutes on the front door or going flat out for the three days, we thank everyone who helped out because every bit counts.

We must also take off our hats to our ladies who not only slave away in the canteen for three days each May but still allow the men to have their hobby for the rest of the year. Thank you for helping to make 1993 another successful exhibition.

While the congratulating is going on, we must also thank some individuals. Firstly, Bob Mawson should be called Mr. Exhibition. Most of the time all of us see is a flurry of arms and legs "There goes Bob" as he races around doing all the jobs that everyone else forgets. There are rumours that this is the way he drives buses, but we can't confirm this. Jim "This is my last year" Christie has again organised the event to perfection. If Bob is Mr. Exhibition then Jim must be Master Exhibition. While Bob is doing all the jobs everyone else forgets, Jim is the one who remembers they need doing. Thanks also to Arthur Hayes for keeping the financial side arranged for both the show and the club. Just remember how it all works so that you can keep South Brisbane the most happening station in the suburban area. Kerry Hayes is another person who deserves mention for the time and effort she puts in to organising the Canteen. Thanks very much Kerry. To the others who haven't been mentioned, don't think that you are being left out. AMRA needs each and every one of you for all of its events whether it be the show, auction, clinic day or any other event and your participation is the guarantee of its success.

Things are still going well in the clubrooms with the new control panel working well. On running days, it would be appreciated if people can be patient when running trains since our controllers are still learning the ropes. It gets a bit hectic sometimes both at the panel and at South Brisbane Central so if tempers become a little frayed, don't take offence, it's just the operators letting off steam. We'd also ask that no work be done on the layout during the

running days due to the headaches it causes for the controller.

Make sure you bring in your goodies at the show and tell in June so you aren't the only one who gets to drool over them. Don't forget MODEL RAILWAYS ARE FUN.

Happy Modelling

Tony Weber
President

Monthly Gathering Report

February

Our entertainment for this evening was Arthur Hayes detailing shunting operations on the Queensland Railways and how it can apply to our modelling enhancing operation enjoyment.

By the end of the session I'm sure we all were a little more enlightened than we were of the sometimes complex situations on prototype railways. Important topics Arthur dealt with included types of wagons, types of draw gear, wagons with/without buffers, types of freight, including placement of dangerous goods, and placement of wagons in train for ease of shunting later on in the trains route.

Arthur explained shunting procedures at various prototypical locations, including his then location of employment at Rocklea station. Every morning Rocklea receives a large freight train from Toowoomba and points west, this is marshalled to suit the contract freight forwarding agent and loaded during the day. Each weekday evening, the train is shunted into the correct consist and departs for Toowoomba and points south to Wallangarra and west to Dalby etc. Motive power is normally a 90 ton Clyde GM Co-Co locomotive from the 1502, 1550 or 2400 classes. The train is normally almost a full load for the range climb, averaging 630 tons and a length of approx 300 m.

Arthur invited interested members to visit his location by prior arrangement on one of the evenings to see the action first hand from a safe viewing point. Many members took up the invitation and enjoyed an evening of prototype shunting action.

March

Don Warn was our speaker at this gathering, his topic dealt with Weathering of Rollingstock on Model Railways.

His methods are derived from much experience and many ideas from modellers around Australia. Don has attended many clinics on the subject in southern states.

The method he showed the gathering gave good results and are water based, so if you make a mess, the item can be cleaned down easily and the weathering tried again. Don uses Pelikan Plaka water colour paints available from most art supply shops for around \$5 a bottle. The main colours used are black, white, red, yellow and brown.

Clubroom News

The Branch Library

Assistant Librarian Matthew Greig has been tied up with his school studies this year and he suggested that another member may be able to assist in the Library. At a recent Monthly Gathering, Ian Moore, another young modeller came forward to fill the position to assist the branch in this interesting and important role. Thank you Ian.

Presently we are looking at expanding the shelves in the Library and suitable shelving is being investigated. Also remember to check out our selection of surplus magazines available at low prices. Note too, the sale of books and equipment planned for our July Monthly Gathering.

A Branch Business Card

Recently the Committee of Management of the Queensland Branch devised a business card for the branch to assist in notifying visitors of our position, phone number and opening times. Unfortunately many people turn up on the wrong Thurs-

AUSTRALIAN MODEL RAILWAY ASSOCIATION Qld Branch Inc.

Meets at
South Brisbane Railway Station
on the
2nd & 4th Thursdays at 7.30pm
and the
1st & 3rd Saturdays at 2.00pm
of each month.

Telephone during club hours
844 1866

day evenings and saturday afternoons and find we are not in operation. These cards are available near the front door and members are welcome to issue these as they see fit.

Clubroom News

As mentioned in the last Journal, the layout wiring system was changed over to the upstairs panel during February.

The first running day was quite an exciting time for most, and busy as well with over 50 attending the running day. The upstairs panel proved itself as a viable system with only operator error being the main problem. Signalling had been installed on the up track, allowing more than one train on that track. The other track has yet to have signals installed, and it is normal to allow only one train on this section at one time.

Except for the signals at Gowrie Junction, all signals are manually operated, however work is being done to install signals with automatic operation as much as possible at all locations, through simple series/parallel wiring a green light will only be given automatically when two adjoining sections are selected to the same controller and points relevant to the situation are correctly set. Presently a signal is installed at the end of the changeover section - watch out for this one. Also remember to change direction in this section. The main controller No 1, the Radio Control Systems unit, needs to be shut down completely for a successful reversal. This RCS controller has proven to be the most popular controller, and consideration is being given to further purchases.

Over the next 6 months, we must stand back and examine the layout operation thoroughly to ensure it lives up to our expectations and needs, before scenery work becomes more advanced. Already there are suggestions of ideas which have come forth from the new panel operation. These include a rehash of track sections and changes to points and sidings which will allow better utilisation of the layout for club members use.

With the new panel operation it was good to see members take an interest in learning its operation. Any suggestions on how it can be improved are welcome. (New track diagram on panel)? Philip Hadley, Robert Farlow and Ian Moore were some of the younger members who took on the challenge to operate the main panel. Their younger minds seemed to pick up the operation better than the older sometimes grey members.

At a recent layout construction afternoon the layout committee held a pick your brains session after smoke to get ideas for the future direction and present progress for the clubroom layout. The session proved fruitful, everyone's ideas were discussed, recorded and several decisions made, these have been posted on paper sheets at the relevant positions allowing further comment. These consultative sessions may be held on a more regular basis.

As with any part of the clubroom layout, our theme is continuous im-

provement. If something can be done better, lets do it again. We are all in a learning process. Please submit your ideas on paper to one of the layout committee members.

Branch Timetable

JUNE

5th Sat	Layout running afternoon
10th Thurs	Layout construction evening
19th Sat	Layout construction afternoon
24th	Thurs 7.45pm MONTHLY GATHERING - Show & tell - see notes below.

JULY

3rd Sat	Layout Running Afternoon
8th Thurs	Layout construction evening
17th Sat	Layout construction afternoon
22nd Thurs	7.45pm MONTHLY GATHERING - Members auction - see notes below.
31st Sat	Layout running afternoon

AUGUST

7th Sat	Layout running afternoon
12th Thurs	Layout construction evening
21st Sat	Layout construction afternoon
26th Thurs	7.45pm MONTHLY GATHERING

SEPTEMBER

4th Sat	Annual Auction at Wavell Heights
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NOTES

At the monthly gatherings, the layout should be available for operation before and after the meeting.

On Saturday 3rd July, the Branch is running a Clinic on the Southside of Brisbane, so the activity at the clubrooms may be limited - check at the clubrooms beforehand.

Monthly Gathering Notes

June - Show and tell

These can be a most interesting evening.

Members are encouraged to bring along anything of railway interest that can be shared with those present in a few moments. This can include your latest model purchase, a repaint, kitbash or scratchbuilding exercise. Perhaps it is a model of historic significance. Members could bring along other railway artifacts, etc of general interest, photos, photo albums, a selection of up to 20 slides, short movie film or video of a few minutes duration. Don't leave home without something to Show & Tell !

July - Members Auction

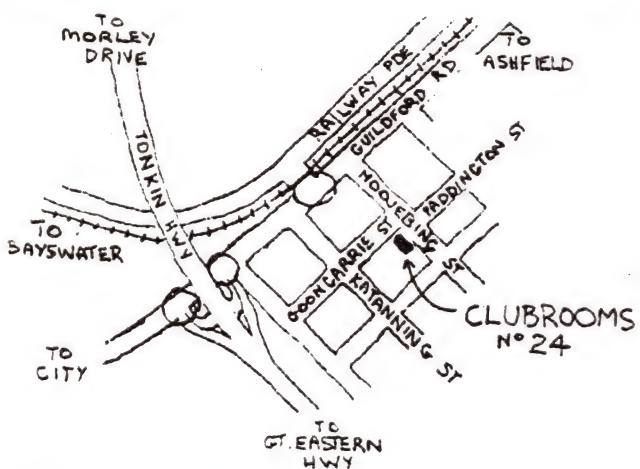
Although these Auctions are not as exciting as our Annual September Event at Wavell Heights, they are well worth the exercise and usually a bargain for everyone. The Branch will be disposing of surplus modelling items and books from the collections of the late Keith Wilcox and Keith Andersen, plus some from the Clubroom collection. Members are also welcome to bring along your own surplus items to submit for Auction.

A Midweek Midday Meeting?

Other AMRA branches and similar groups have found that a Midday, Midweek meeting is popular and beneficial. It suits retirees, shiftworkers and other members who have time off in the midweek. These events are called all types of names from "Gentlemen's Days", Daylighters and other not so polite names. These prove to be great opportunity for a bit of quiet layout operation, general chit chat, library reading and other activities.

Jim Hoskins is interested to hear from other Queensland Branch members who are interested in this idea. Jim can be contacted on 07 208 5288. The clubroom layout is usually available for operation in the first and last weeks of the month, Jim and those interested will be given training in the new Control panel, we to have made as simple to operate as possibly can be done, but these always room for improvement.





Simon's Scribblings

Meritorious Award for David Port

My apologies to David for not having included news of the Meritorious Award in my last lot of "Scribblings" - it was an oversight on my part and was only noticed by our Editor & Publisher after he had printed the February issue. Yes, it was my great pleasure to present the A.M.R.A. Meritorious Award to David Port on behalf of the Federal Committee of Management, at the Branch's 1992 Christmas Wind-Up on December 19th last.

David joined A.M.R.A. in July 1989, towards the end of our occupancy of Meltham station, little thinking that he would spend much of that year helping with the finishing touches of the construction of the new Clubrooms at Moojebing Street. David and his wife Alsema were welcomed to the Branch in the August 1989 issue of "THE BRANCHLINE" and the words used there bear repetition now..... "David has already contributed to the Branch by way of a donation to the Library's stock of reading and reference material. He has been observed in active service as a brick cleaner and a roofing carpenter - an example to many of our 'older' members".

He has continued to be a tireless worker in and around the Clubrooms and his skills were numerous. When the dust had settled from the Clubroom finishing activities, he became a glutton for punishment by becoming part of the team building the original "Ebford Regis" 'O' scale layout. He is also busy building his own layout in addition to scratch-building locomotives and, believe it or not, now busily involved in building "Ebford Regis" Mk.II and some of the structures on that layout. He is usually one of the first members to arrive on "Busy Bee Days" and gets stuck in, without any fuss, to whatever work has to be done.

David is a quiet, unassuming member of the Branch who has, by his efforts, proved that you only get out what you put in. He has put in a lot and his satisfaction with the Branch and with railway modelling has been his reward. He is an example to others and the Meritorious Award is a very appropriate reflection of the esteem in which he is held.

1993 Model Railway Exhibition

Preparations are proceeding apace for this year's Model Railway Exhibition.

Please do what you can to assist in making this year's Exhibition a great success. It is the Branch's major source of

income for the year. Most of our activities in the Branch and in the Clubrooms rely on the Exhibition for their funding.

Any time that you can make available to assist with the running of the Exhibition during set-up, while we are open to the public and during the packing up will be most appreciated. Please also, if you would, sell the raffle tickets sent with the last issue of "THE BRANCHLINE". While this is a valuable fund raiser, we appreciate that not everyone finds it easy to sell raffle tickets. Therefore there is no obligation to do so. However, there IS an obligation to return the butts and/or the unsold tickets to the Branch as they all have to be accounted for.

1993 Annual General Meeting

Make a note in your diary or on the calendar now - the Annual General Meeting this year is on Monday, 5th July, starting at 8.00 pm.

The Western Australian Branch 'Club' Award

The Management Committee is seeking nominations for "Clubman of the Year" to receive the Ted Thoday Encouragement Award, now known as "The Western Australian Branch "Club" Award". Put on your thinking caps, work out who you think has made an exceptional contribution to the Branch's activities during the past twelve months and then let Eddie, or any of the Management Committee, know, preferably in writing, who your nominee is. The Management Committee will then sift through the nominations and select "The Clubman of the Year" for 1993 and this will be announced and a presentation made at the Annual General Meeting.

Simon Mead
President

Membership Matters

Nine new members who reside in Western Australia, have joined

since the last was published.....well, not quite true, as three of them are former members rejoining. All are automatically members of the Western Australian Branch as a consequence and we are pleased to welcome them 'on board' (and back!). We hope that they will find membership of the Association, in general, and of the Western Australian Branch, in particular, to be enjoyable and beneficial to their modelling aspirations.

John BAJKOWSKI Merredin 'Sn3 1/2'
* Craig BELCHER (son of Ken) Embleton
* Barry BRYANT Armadale 'N'
Paul & Felicity MEADWELL Huntingdale 'N'
Shane MILES Jarrahdale 'N'
Bernard MINCHIN Karratha 'N'
Charles STEWART Eden Hill 'HO'
* John WHITE Nollamara 'On2' (0-16.5)

The three former members rejoining, after breaks of up to eight years, are identified with an asterisk (*).

These additions bring the total membership of the Branch up to 334. At the beginning of February, when the Branch strength stood at 325, there were 1,009 members of the Association, Australia wide, so it can be seen that the Western Australian Branch makes up 32% of the membership of the Association.

New members of the Branch for 1992/93 (actually since and including the 1992 Model Railway Exhibition recruitment) now number 84, which is still more than all the other Branches of the Association put together! Must be something we do.....

Alan Porter
Branch Registrar

Fly on the Wall

At the Management Committee Meetings

JANUARY: Expenditure of \$590 approved for "Ebford Regis" for the first quarter 1993. Approval given for purchase of four more American videos, two based on the prototype and two featuring U.S. model railroading. Purchase of a further two videos from Peco Publications also approved. Alan Porter volunteered to take over the role of 1993 Model Railway exhibition Co-ordinator following the relinquishment of this role by Lionel Boyce due to ill health. Admission prices for the 1993 Exhibition agreed at \$4.00 for adults, \$2.00 for school age children and \$3.00 for holders of all types of Government concession cards. A vote of thanks given to Alan Porter for his excellent work in constructing and installing the latest set of shelves in the Library.

Investigation continues to find a new supplier of Branch name badge holders. Air-brush spray booth and noise from compressor being looked at in order to improve Layout Room conditions.

FEBRUARY: Photocopier to be serviced as soon as possible. Various measures to alleviate conditions in Layouts Room continue to be considered. Donation by Jim Hidden of a video featuring the construction of the new Northern Suburbs Railway. Verbal advice from the W.A. Model Collectors Club regarding their Open Day at Gosnells on 16th May but decision on Branch or members' private involvement awaits receipt of letter with details of space available, etc. Approval given for acquisition of eight modelling books from Wild Swan Publications to supplement those donated by Ted Thoday in the past. Agreement reached on conditions for use of Clubrooms on a bi-monthly basis by the Southern Railway Modellers Group. Similar offer to be extended to the British Transport Modellers Circle. Benefits of membership of FLAG (Federation of Leisure Activity Groups) being assessed. Progress report on preparations for 1993 Model Railway Exhibition reviewed and raffle noted to be now back on track with first sales of tickets to commence on 13th March and for members to receive their initial allocation of tickets with the April issue of "THE BRANCHLINE". Space rental rates for traders at the 1993 Model Railway Exhibition agreed at \$6.00 per sq. metre for a 'standard' 60 sq. metre shop and \$7.00 per sq. metre for more flexibly sized and positioned stall.

Reader's and Viewer's Corner

Generous donations of reading matter of various sorts have been received in the past two months from Barry Keens, George Foster, Jack Elliott, Gil Reid, Bill Williams, Ted Thoday ("Build a Model Railway") and John Neville, as well as from non-members Paul Kehoe, Ron Dyer and Jim Preston (of P&J Models). Thank you, all; your generosity is much appreciated. Most of the donated items have still to be checked against the holdings in the Library and, if not already in our collection, to be catalogued. So, please be patient and please do not take books, magazines, etc. out of the boxes in which these donated items are stored in the Library pending these actions.

Seven new videotapes have been purchased and all are now available for members to use. They are:-

"Creating Classic Layouts" Volume 2: Moving Mountains, featuring three Continental classics, "Jura Simplon", "Charmes" & "Europ".

"Creating Classic Layouts" Volume 3: Prototypes Portrayed, featuring two contrasting classics, "Lynbridge" & "Chee Tor".

"Modelling my Favorite Railroad" - The Rio Grande Southern.

"Sierra Pacific Lines" of the Pasadena Model Railroad Club.

"The East Broad Top", the ex-coal hauling 3' system in Pennsylvania.

"CP Rail's Rogers - The Last Pushers" - slogging through the Selkirk Mountains in Canada.

"North Shore 1945" - the Chicago, North Shore & Milwaukee RR.

An order has been placed for eight Wild Swan Publications Ltd. modelling books to supplement the several titles from the same publisher that Ted Thoday has donated over the past three years. The titles are: "Landscape Modelling" by Barry Norman "Architectural Modelling in 4mm Scale" by Dave Rowe

"Light Railway Layout Designs" by Iain

Rice

- "An Approach to Building Finescale Track in 4mm" by Iain Rice
- "The 4mm Wagon, Part One - Opens, Minerals & Hoppers" by Geoff Kent
- "Locomotive Kit Chassis Construction in 4mm Scale" by Iain Rice
- "Great Western Branch Line Modelling, Part One: Prototype Layouts, Track & Signalling" by Stephen Williams
- "Great Western Branch Line Modelling, Part Two: Prototype Buildings, Fittings and Traffic Operation" by Stephen Williams

We also have put our name on the list of those awaiting a reprint of Dave Rowe's "Industrial and Mechanised Modelling".

These titles will be very useful additions to the six titles donated by Ted. We are also looking at the modelling books we have that were published by Patrick Stephens Limited with a view to adding some more of this publisher's titles. As they say, "we'll keep you posted!"

Finally, furrowing my brow, I ask members to:-

(a) try to keep all the items on the Library shelves in their correct order, the books in particular. It's quite easy to do and it makes the Library so much more 'user friendly' for other members.

(b) see if any of the eight books still missing from the Library are in your possession. Three of those listed in the last issue of "THE BRANCHLINE" have been returned anonymously and we thank whoever it was that had those books for their action. Now for the other eight missing books, which I list again below.

A001 Model & Miniature Railways P.B. Whitehouse

D103 MRC Special No.3: Baseboards and Trackwork

D109 Instant Trackage in 'N' - Layout Plans for Table Top and Baseboard

D120 Hornby Track Plans, 6th Edition

E203 Scenery for Model Railroads Bill McClannan

E101 Model Railway Signalling and Point Operation

E102 Practical Electronics for Railway Modellers, Pt. I R. Amos

J304 Index to Model Railway Drawings S.A. Leleux

Return of J304 is most earnestly sought. Who's got it?

Read my notes in the last issue regarding anonymous return of these books.

**Eddie Garforth
Library Co-ordinator**

The Western Australian Branch 'Club' Award

The Branch Management Committee will again present one of the members of the W.A. Branch with 'The Western Australian Branch "Club" Award' in recognition of an outstanding contribution in 1992/93 to the betterment of the Branch.

The perpetual trophy associated with this Award is The Ted Thoday Encouragement Award and, after agreement with Ted on the change of the conditions for its use, was awarded in 1991/92 to Dennis Ling. Dennis received a replica trophy as will the member to whom the Award is made this year.

Members of the Western Australian Branch are invited to nominate a member

of the Branch who they consider has contributed to the betterment of the Branch and whose efforts deserve to be recognised formally. A written nomination should be submitted to the Branch Secretary no later than Saturday 22nd May 1993. The nomination letter should include a paragraph outlining the reasons for the nomination.

At its May and June meetings, the Management Committee will consider all the nominations received and will decide by secret ballot, the winner of 'THE WESTERN AUSTRALIAN BRANCH "CLUB" AWARD' at the latter meeting. The announcement of the Award will be made at the Annual General Meeting on Monday, 5th July 1993

How I began in Model Railways

by John Neville

At no point that I can recall did the idea of model railways suddenly grab me; I seem always to have had, like Christopher Robin, "a train upstairs" or, at any rate, close at hand. However, my earliest experience of a genuine model railway was probably when, in 1919, I first saw Roye England's layout at his home in West Perth. (You've never heard of Roye England of Pendon? Well, that's another story, of great model railway interest but too long for now.) Our families were friends and neighbours, so I often visited the railway, but the age gap between my six years and Roye's eleven limited my participation in actual operations mostly to the "stand over there and don't touch anything" variety.

The layout circled the back lawn (where dentists' cars are now parked; surprisingly the house still stands and I was able to send some photos of it to Roye recently). The line was in Gauge 'O' and there was a station and a bridge over a baking-tin lake inhabited by fierce tadpoles and a tunnel through a mountain fully half a metre high. I remember only one train, a clockwork locomotive and several tinplate coaches - probably Marklin - in the livery of the English pre-Grouping L&NWR, Roye having not then fallen under the spell of "God's Wonderful Railway" and the Vale of the White Horse that inspired Pendon. (Pendon? See the reference in paragraph 1.)

As a matter of fact we then thought GWR locomotives were rather sissy; we tended to favour the high footplate, outside valve motion Pacifics of the Great Northern Railway like "Flying Scotsman", for instance, which by a little imagination-stretching could be identified with our own familiar but more humble local motive power. "Flying Scotsman" remains Number One in my affection, but when Roye set eyes on the real thing, he was seduced by the products of Swindon to the exclusion of all others.

In those old days in Perth, our railway interests were almost entirely British orientated. The WAGR followed British practice to a great extent and we naturally took plenty of interest in its operations, but unnamed, dirty, black engines and drab red-brown rolling stock just couldn't compete with the colour and the glamour of England's trains and their high speed performance, politically unfashionable though it may currently be to speak favourably about anything British! We saw quite a lot of American railways at the cinema, of course, but somehow they were more associated in our minds with cowboys and Indians than with model railways - although I must confess that at one time a

couple of us viewed an absolutely sickening romantic drama called "Love Never Dies" about six times because of a particularly spectacular sequence of a train plunging off a trestle bridge into a ravine (the heroine was in the last coach, which naturally stayed on the rails!) - a scene which we tried to reproduce with our own trains - not the crash itself, of course, but the aftermath.

My own first train was a Brimtoy (anyone heard of them?), an 0-4-0T in

Great Northern style with a couple of coaches. Operating scope was limited, for the loco had no reverse gear or brake, but it had a distinctly Ivatt atmosphere about it and for a long time helped to visually boost my loco stud.

By now my reader (if one such has persisted this far) may well have come to some disparaging conclusions about my age and possible senility with this talk of LNWRs and GNRs and so on, but these almost prehistoric lines are an essential part of how I began not only to become involved in model railways but also to develop the keen interest in the real thing, which seems to me essential to get the fullest enjoyment out of modelling.

A visit to England with my parents in 1920 and many journeys by rail there, some behind the resplendent blue and gold of the Great Eastern but mostly on the East Coast Joint route to the North, generated my interest in railways generally, and a firm affection for those of England in particular, which I retain to this day.

On our return to Perth, my Brimtoy was soon joined by a Hornby No.2 Goods Set (what would we have done without Meccano Limited?). This was ostensibly GNR, in that the loco was green and the two open wagons had GN on the sides, but of course the old No.2 locomotive came in green, red or black with rolling stock lettered GN, GW, MR or LNW to suit; it always, however, bore the number 2711.

While not being quite as dismantlable as the early advertisements implied, the tender and the wagons clearly showed their Meccano affiliation by being attached to their undercarriages by Meccano bolts and nuts. The undercarriages themselves were very basic, with solid triangular trunnions carrying the wheels and axles. The latter were of about 4mm tube with closed ends on which the pressed tinplate wheels were loose mounted, being kept to gauge by pressed out lugs. All these features disappeared from Hornby products soon after this time, later models having more detailed frames and more conventional wheels and axles, and now people tend to look disbelieving when the earlier ones are described.

By now I had become somewhat single-minded about railways and when asked what I wanted for birthdays and for Christmases I always had a prompt and precise answer. However, my hopes were not always realised: as many children know only too well, adults tend to give the presents they fancy themselves giving, so that hoped-for signal gantry or side tipping wagons all too often turned out to be a nice blue jumper or a book about birds.

Nevertheless the stock grew, but the layout didn't, having to be assembled for use and put away afterwards. Then came the almost unbelievable realisation of a dream; we moved to the Hills and I was allocated a space in the new garden the size

of a tennis court to do with what I willed - and I did!

And that's how I began in model railways. The story is based mostly on memory, but the extensive layout that developed over the period of about six years following is fairly well documented. For the last four years in particular there was some press publicity and a detailed monthly log was kept, including diagrams and photographs, which I still possess. But that's yet another story.

1993 Model Railway Exhibition Report

The 'count-down' for the 1993 Model Railway Exhibition has started. The formal letters of invitation to potential exhibitors, to kindred societies and to traders in model railway and related items have just gone out and the publicity machine has its engine running and is about to move off. Members can help here by placing as many handbills as they can in the hands of people that they think might well come to the Exhibition if they know about it. There are plenty of handbills available so don't stint yourselves in your efforts to promote the Exhibition.

Barry Keens has placed all our press advertising and will later on be sending a 'Press Release' to all media outlets in the hope that they will give us a free mention from time to time. Eddie Garforth is confident of getting some editorial cover in the free issue community newspapers but if any member with inside influence with the print and the electronic media would like to drop a word in the right place, I am sure that Eddie will be quite pleased..... and so will I!

Selling raffle tickets isn't something that I relish, and I am sure that there are other members whose "1993 Model Railway Exhibition Pack" includes a raffle book may feel the same. I usually just shell out \$10.00 and buy the tickets myself - I never know when I will get lucky and maybe the grandsons will get their model railway a bit earlier than is happening at the moment! But, if you have received a raffle book and you don't want to sell tickets or buy them yourself, why not consider making a donation of \$10.00 to the Branch. After all, there is no membership fee to be a member of the Branch and you get a fair number of benefits, such as "THE BRANCHLINE" sent to you through the post!

Oh yes! A late thought on publicity. We have a number of triangular 'roof rack' signs which have been carried on members' cars for about a month prior to past Exhibitions and are a very eye-catching means of promoting the Exhibition. They do not effect fuel consumption, as far as we can tell, and if any member would like to have one of these to put on their car throughout May, please let me know as soon as possible.

The last word....it's your Branch that's running the 1993 Model Railway Exhibition, so please do your utmost to make it a success by following the suggestions in the letter from Simon Mead in your "1993 Model Railway Exhibition Pack".

Alan Porter
1993 Model Railway
Exhibition Co-ordinator

Around the Layouts

Work continues on "Ebbford Regis", both at the Clubrooms and 'off site' at various members' homes. By the time that you read this, the layout may have gone through its "now you see it; now you don't" trick. Dennis Ling, the 'Gaffer' tells me that his gang plans to disconnect the various modules, move them out to the car park, re-assemble them there and then, on the same day, repeat the operation to end up back in the Layout Room. If you blink, you might miss it! There is not a lot of time left before the 1993 Model Railway Exhibition at which "Ebbford Regis" is due to be shown but the layout is coming together well and IT WILL BE THERE. "Father Nature"-(Richard Smart) has been putting in many hours on the landscaping. The trees and grass are growing well under his influence.

If you've been to a Friday evening 'N' scale Special Interest Group gathering, you will find that they're a very laid back affair. It is a case of "do what you feel like". Run some trains on the 'N' scale layout, watch videos (I try to keep them on a rail theme - although it was a great dragster tape, Colint!), browse in the Library or just sit around and yarn. It's a nice way to finish off the working week. If you haven't been yet, why not? Future N Scale Special Interest Group nights will follow the same format but with the addition of some module construction. It is planned to rest the "West-N-Trak" layout from this year's Exhibition and to come back, better and bigger, in 1994. So, there are twelve months to construct some new modules for this layout. If you would like to know how to build an 'N-TRAK' module, come along. I can even supply you with all the materials you will need, at a very low cost.

The "Ossie Gully" 'OO' scale layout doesn't often get a mention when speaking of the Branch's layouts, but it does this time. The layout was tidied up by Steve Price for last year's Model Railway Exhibition where it was very popular as a 'U-Drive'. This year it will be there again in the same role but with a little added attraction. The locomotives and rolling stock will be very familiar to the younger ones - the star of the show will be a little blue tank locomotive with a face where his smoke box door should be. Go to the end of the queue please, kids!

Ian Wood
Layouts Co-ordinator

Programme

Mon 17th May	General Club Activities
	Note 1
Wed 19th May	Sn3 1/2 Special Interest Group Assembly
	Note 4
Fri 21 May	'N' Scale Special Interest Group Mtg.
	Note 3
Sat 22 May	General Club Activities
	Note 1
Wed 26 May	Exhibition Preparations
	Note 5
Sat 29 May	Exhibition Preparations
	Note 5
Mon 31 May	Exhibition Preparations and Briefing
	Note 6
Wed 2 Jun	Exhibition Preparations and Loading of Barricading and Other Equipment
	Note 7
Fri 4 Jun	Set-up Exhibition
	Note 8

Sat 5 Jun 1993 MODEL RAILWAY EXHIBITION Note 9
 Sun 6 Jun 1993 MODEL RAILWAY EXHIBITION Note 9
 Mon 7 Jun 1993 MODEL RAILWAY EXHIBITION including Take-down of Exhibition Note 10
 Wed 9 Jun Return of Barricading and other Equipment to Clubrooms Note 11
 Fri 11 Jun 'N' Scale Special Interest Group Mtg. Note 3
 Sat 12 Jun Special Project Afternoon Note 12
 Mon 14 Jun Sn3 1/2 Special Interest Group Meeting Note 2
 Wed 16 Jun General Club Activities Note 1
 Fri 18 Jun 'N' Scale Special Interest Group Mtg. Note 3
 Sat 19 Jun "No Reserves" Auction Note 13
 Mon 21 Jun 1993 Model Railway Exhibition Post Mortem Note 14
 Wed 23 Jun Sn3 1/2 Special Interest Group Assembly Note 4
 Sat 26 Jun General Club Activities Note 1
 Wed 30 Jun Mini Talks Note 15
 Fri 2 Jul 'N' Scale Special Interest Group Mtg. Note 3
 Sat 3 Jul General Club Activities Note 1
 Mon 5 Jul Branch ANNUAL GENERAL MEETING Note 16
 Sat 10 Jul New Members' Introductory Afternoon Note 17

Programme Notes

All meetings will be held in the Branch's Clubrooms at 24 Moojebing Street, Bayswater, right opposite Paddington Street. Moojebing Street runs off Guildford Road towards the Swan River - about 50 metres east of the traffic signals which are opposite the Cresco fertiliser works.

The times of the meetings are as follows, unless otherwise noted:-

Mondays and Wednesdays: 8.00 pm
Fridays: 7.30 pm Saturdays: 1.30 pm

The Committeeman who is the Duty Officer for the meeting will open the Clubrooms at least 15 minutes earlier than the times specified above.

Visitors are always welcome - just come on in and introduce yourself to the Duty Officer (ask someone who he is). He will make you feel really 'at home' by showing you around the Clubrooms (which we know are the best of any model railway club in Western Australia and possibly even anywhere else in Australia) and by introducing you to some of the members present.

The fees payable by members at each meeting cover some of the general operating expenses of the Branch's Clubrooms and entitle members to hot refreshment (tea, coffee, Milo) and biscuit.

Note 1 At General Club Activities meetings, you may use the Clubrooms more or less as you wish - you can run your own trains (or use some of the Branch's equipment) on the 16.5mm gauge "Haltwhistle" layout **WITH CARE**, you can do similarly on the 9mm gauge "West-N-Trak" layout **WITH EVEN MORE CARE**, you can try your hand on the Shunting Puzzle on the "Ossie Gully" layout, you can assist with the development of the 'O' scale "Ebford Regis" layout, you can browse through the mountains of reading

matter that we have in the Library, you can put a videotape in the VCR (and sit back and watch the show), you can use the Branch's 'Work Station' for that little job you can't do at home (except that you are required to have appropriate accreditation for the air-brushing and the lathe facilities) or, if all these activities are a bit too arduous, you can chat away to like minded model railway enthusiasts.

Note 2 At these 'workshop' sessions, you can try your hand at building a model of a WAGR 3'6" gauge vehicle in 'S' (1:64) scale - you can start, under the guidance of mentors, on a fairly simple project, such as a 4-wheel GC open wagon and graduate, when you're more confident, into more complex goods wagons, passenger rolling stock and steam and diesel locomotives. You can get most of your modelling requirements for such activities, either from "Paddington Market" (styrene sheet, MEK, soldering flux, etc.) or from the Railwest Models sales person present (wheels, compensation units, brass rod, etc.,etc.)

There will be modellers at all stages of skill and with their models at all stages of completion, so you will not be at all 'out of place' whatever your skill level. There's a great group of Sn3 1/2 modellers there to inspire, guide, cajole or otherwise encourage you to get into and excel in this most desirable aspect of railway modelling - doing your local scene!

Note 3 The 'N' Scale Special Interest Group meetings are designed to allow active modellers in 'N' scale to enjoy same running on the Branch's "West-N-Trak" layout, as well as putting some finishing touches to it and to planning the construction of more modules to enhance the size and the interest of this layout. However, you don't have to be a modeller in 'N' scale to come along to these meetings - you just need good eyesight!

Note 4 Slightly different gathering of the Sn3 1/2 modellers who meet on the second Monday of each month (except in June, where there is a conflict with the Model Railway Exhibition). On this second occasion each month, there will be a short formal session when there may be a showing of suitable slides on WAGR subjects to illustrate same particularly topical point, a short talk on a topic of modelling relevant to Sn3 1/2 modelling or a showing of newly available modelling materials and items suitable for Sn3 1/2 modelling of the WAGR 3'6" gauge system.

Following the short formal session, the normal 'workshop' session of an Sn3 Special Interest Group meeting will prevail.

Note 5 Apart from preparing two of the Branch's layouts for the 1993 Model Railway Exhibition, the "Ossie Gully" layout is more or less ready but the stock to be used needs to be checked out, the 'O' scale "Ebford Regis" layout needs all the help it can get!

There's a vast number of 'infrastructure' items used by the Branch to put on a good Exhibition that need to be checked out and, if necessary, repaired and rehabilitated. The list includes the barricading (generally, ready to go), the ticket boxes, the visual screens, the display boards, perhaps the A.M.C. Modules, signs of all types and descriptions...the list goes on and on!

And there's the admission tickets to be numbered, although it is hoped that most of them will have been done on General Club Activities meetings in April and earlier in May. Anyway, come along and join

in the pleasure of a team effort towards a common goal.

Note 6 A little time will be taken off this evening for members who are participating in the 1993 Model Railway Exhibition (either as an Exhibitor or as an A.M.R.A. 'official') to receive a briefing on a number of points which will help us to put on a good Exhibition for the public, as well as enjoying it themselves.

Note 7 A good turn-out of members is sought to help carry the barricading piping, fittings, wooden blocks and other odds and ends from the Clubrooms and load it all onto the truck, as well as preparing the 'flats' such as the ticket boxes, the 'BP' screens and the display boards for early pick-up on the following Friday.

The "Ebford Regis" 'O' scale layout has also to be readied for removal to the Silver Jubilee Pavilion, and possibly loaded onto a truck for delivery to the Pavilion the following evening. Details of the transportation of this layout will be posted on the Clubrooms blackboard as soon as possible.

Note 8 There's a lot to do in setting up the Exhibition apart from the exhibitors bringing along and setting up their displays. Transportation and erection of the 'infrastructure' items, erection of the barricading, possible transportation and, certainly, the erection of the "Ebford Regis" and the "Ossie Gully" layouts... They've all got to be done by someone. Volunteer yourself for all or part of this day, either at the Silver Jubilee Pavilion as an 'erector' or at the Clubrooms as a 'loader and carrier' (if you have a trailer, it could be useful). Activities will start early (9 am or before at both locations) and will carry on until it's all done, hopefully early in the evening.

Note 9 Lots of member involvement is needed to 'run' the Exhibition and we hope that all members will be able to help sometime over the weekend. Use the Roster Form to indicate when **YOU** can. You'll get to see the Exhibition for free if you do. Otherwise, if you cannot help to run the Exhibition, do come along to see it... but you'll have to pay to get in and you may get a bit of 'stick' from those members who are helping to run the Exhibition.

Note 10 Apart from the involvement described in Note 9, there's the need to dismantle the barricading and other 'infrastructure' equipment and to load these things onto whatever transport we have for removal from the Silver Jubilee Pavilion this evening if possible. Otherwise, some of us will have to come back on the following day and we're not too keen on that! So, again, lots of help is needed and if you've got a trailer, you may be able to assist with the transportation. Volunteer yourself for this work (the Roster Form has a section this year for this activity) as many hands will make light work!

Note 11 The barricading and some of the 'infrastructure' equipment will have been 'taken home' on Monday evening by members with trucks and trailers and, tonight, it will be brought back to the Clubrooms for return to storage NEATLY

There's also the re-erection of the "Ebford Regis" 'O' scale layout which was taken down to the Showgrounds for display.

Note 12 Assuming that your Editor can thump enough keys on his typewriter between the end of the Exhibition and today, we'll spend the afternoon 'persecuting the paper' of the June issue of "THE BRANCHLINE". Collating, stapling, checking, folding, enveloping, Post Code sorting,

etc. - there's a job to suit everyone's talents!

And for those who are not able to find a job in the paper persecution department, there's sure to be some tidying up of the Storeroom and other areas following the return of the equipment from the Exhibition and there will almost certainly be more to be done in getting the "Ebbford Regis" layout up and running again!

Note 13 Only A.M.R.A. members may submit items for sale at this Auction but nonmembers will be welcome as potential buyers. The Auction Co-ordinator will give consideration, if requested, to using this Auction as a means of disposing of any model railway items contained in the estates of deceased persons who were the friend of an A.M.R.A. member but not a member themselves.

The main rules of the Auction are printed on the reverse side of the Auction Form, which will be available at the Clubrooms from Wednesday, 2nd June.

As THIS IS AN AUCTION WITH NO RESERVE PRICES ON THE LOTS TO BE OFFERED, there will be no need to fill in Column (c) on the Form and, of course, Rule 5 will not apply on this occasion.

It is preferred that successful bidders make full payment 'at the fall of the hammer' (and thus enable them to take delivery of the Lots knocked down to them) but it is realised that buyers may not have brought sufficient ready cash with them to cover their purchases or have forgotten to bring their cheque books with them. To assist in these circumstances, the Branch is prepared to accept a deposit of NOT LESS than 10% of the amount owed to the Branch. The remaining amount must then be paid by Saturday, 26th June. The deposit will be forfeited if the remaining payment is not made by that date and the Lot/s concerned will become the property of the Branch.

Unsold items will be returned only to the vendor, or if they are sold by private arrangement after the Auction (but still within the Clubrooms), they will be given to the buyer but the Auction Co-ordinator MUST be told of this by the vendor. In this case, the normal 10% selling commission to A.M.R.A. will still apply. There is no limitation on vendors making private arrangements with buyers outside of the Clubrooms, either before or after the Auction and in this case there is no commission payable to A.M.R.A..

The Clubrooms will be open from 1.00pm to allow for early marking up of the items for sale and their display to potential buyers. Vendors are encouraged to complete their Auction Forms before they come to the Clubrooms on the day of the Auction. **NO LOTS WILL BE RECEIVED AFTER 2.00 pm.** The Auction will commence at 2.15 pm and no later.

Note 14 Tonight, we'll hear how things went at the 1993 Model Railway Exhibition and then each of us can offer constructive comment on what happened and how we can improve our performance for 1994. We'll never improve unless we get feedback from members and others.

Note 15 Ted Thoday, Barry Keens and Alan Porter are provisionally the presenters of a 'mini talk' each. It is expected that each 'mini talk' will last 15 to 20 minutes.

Note 16 See the Notice below.

Note 17 A special afternoon to help new members and/or beginners in the hobby - there will be a showing of an appropriate

video for beginners and there will be a panel of experienced modellers to help with problems being encountered. New members will be given guidance on using the Library, on operating the Branch's model railway layouts and on the sales facilities available to members at "Paddington Market" and at "Moojebing Market".

The 1993 Annual General Meeting

Members are advised that the 1993 Annual General Meeting of the Australian Model Railway Association, Western Australian Branch Inc will be held at 8.00 pm on Monday, 5th July 1993, in the Clubrooms, 24 Moojebing Street, Bayswater.

Members wishing to have any matter placed on the A.G.M. Agenda are requested to provide details to the Branch Secretary no later than

The formal notice for the 1993 Annual General Meeting, the Agenda for the meeting and a Nomination Form for Officers and Management Committee election will be included in the June 1993 issue of "THE BRANCHLINE", due to be mailed out to members on Monday, 14th June 1993.

Book Review

"BUILD A MODEL RAILWAY"

by Dave Lowery Argus Books 210mm
x 145mm 128pp ISBN 1
85486 045 3 UKL6.95

As part of a promotion to introduce the new format 'Model Railways' magazine, Argus Specialist Publications offered a copy of this book in exchange for tokens attached to the first three issues of the new format magazine. Your reviewer accepted the offer and has now donated the book to the Branch Library.

The author is a very experienced railway modeller and is a former Editor of "Model Railways" magazine. He takes the novice modeller through every stage of producing an operating model railway layout, from an initial discussion on such basics as why use a baseboard, choosing a scale/gauge combination, through to layout design and construction, providing buildings and rolling stock.

To achieve this, the author has used an Argus Publications layout plan which itself is based around the Hornby track and pointwork system. A component list is given.

A well written, uncomplicated book. A complete novice with basic skills and a few tools could work through this and end up with an operational model railway layout. An excellent book for anyone new to the hobby but of use to the more experienced modeller as well!

Well worth the purchase price!

The author makes reference to a videotape entitled "How to Build a Model Railway" in which he participated - the video adds pictures to the words and the still pictures in the book. This video is available in the Branch Library, Ref. No100V23.

BET

Have You Seen?

MODEL RAILWAYS" February:

'Test Track' looks at the Bachmann '00' scale model of the SR 'Lord Nelson' 4-6-0.

Foxhunter Models 'N' scale kit for a Robin-son 2-8-0 is reviewed.

'Show Piece' looks at "West Jayton", a 4mm scale 'EM' layout based on MR. Karen Grantham continues her 'Paint Shop' series - weathering locomotives. An infra-red control system described. A freight vehicle repair yard as an alternative to a goods yard. The 'Soldering' series is continued - putting it all into practice by starting on an etched metal wagon kit. Researching the prototype before constructing 4mm and 7mm scale models of the LNER K3 Class 2-6-0. Making your own lining transfers. More on "Ilkeston Town" featured in the January issue.

"MODEL RAILWAYS" March:

Hornby are due to release three versions of the LNER A3 Class 'Flying Scotsman'. New, improved GEM lever frame; a range of new releases planned from Bachmann, Dapol, Lima and Peco in 4mm scale and from Graham Farish in 2mm scale are listed and illustrated. Unwinding with a model railway - "Ambersham" as one such layout is described. Review of the London Road Models 4mm scale etched brass kit of the LNWR 'Special Tank'. A look at the Graham Farish factory shows how they make a complete set locomotive valve gear in two minutes or a complete locomotive chassis in four minutes! More from the 'Soldering' series - whitemetal soldering. The 4mm and the 7mm scale locomotives in the February issue are started. An 'O' scale American narrow gauge in a cameraman's case: you have to see this most unusual layout. Making hedgerows from everyday household items. An engineman's memories of the Kingsbury Branch, something a little different for a model. The 'Paint Shop' series examines the weathering of wagons.

"RAILWAY MODELLER" March:

'Railway the Month' is "Hartwell", a 4mm scale, 'EM' gauge exhibition layout based on the LNWR station at Hartington on the line from Buxton to Ashbourne - lovely modelling and lovely locomotives and rolling stock. Scratch-building a 4mm scale model of the goods shed at Cardigan

A really superb model of a real place, Royton Junction, just north of Oldham on the ex LYR between Manchester and Rochdale, as it was in the 40s and the 50s - it's only a diorama 4' X 4' but the amount and the quality of the modelling is top stuff. Prototype information and drawings of the Brecon & Merthyr Railway 'Stephenson' 2-4-0T. Another exhibition layout, "Brushford" which has been based on Dulverton on the GWR Taunton to Barnstaple line, thought by many to be 'EM' gauge but in reality 16.5mm gauge! Part Two on modelling the Merseyrail EMUs deals with the bogies, glazing, finishing and painting. Improving the way in which R-T-R bogie coaches run and get rid of the wobbles. Part Three of the article on building the 'stowable' 'N' scale layout - the wiring of the layout and of the control panel. A description of the East Midlands Freight Training Module built for BR based on Pye Bridge. Changing the wheels on a Tenshodo 'SPUD' unit. The West Sussex N Gauge Society's modular club layout and how it has been a successful exhibition layout. Combining parts from a Cambrian Models underframe kit with parts from two Hornby wagons to construct a PAB 4-wheel 'Covhop' hopper wagon. Construction of an 'O' scale MR signal box based on Gloucester Eastgate, using styrene sheet.

"Clifton Junction", the 4mm scale layout of the Clifton College Prep. School as told by the master responsible for getting the whole thing going. A student 'OO' scale layout based on Lambourn and using sectional track to standard British geometry. Another student 'OO' scale layout, "Stonebridge", on a 8' x 4' board. Latest reviews and 'News Special' cover seven pages. Among the books reviewed is "The Art of Soldering" by R. Brewster, who is a manufacturer of soldering equipment, some designed specifically for the modeller.

"MODEL RAILWAY JOURNAL"

No.60:

Trevor Potts' 4mm scale model 'EM' gauge model of Churston in Devon is described. 'Small Suppliers Forum' notes that former Premier/M&L Adpack and Microrail kits are available from Alan Gibson; source of O1 Scumble; POW SIDES latest releases; location of Scumble; cordless, gas-powered soldering iron; Duncan Models 7mm scale corrugated sheets and a GWR Lamp Hut; CGW Models GWR oval cabside plates; C&L Finescale catalogue and 4mm and 7mm track components; soldering iron controller; foliage materials for various species of trees. Pre-war petrol depots, design and operation - suitable for that odd corner on the layout. Chassis design and construction for a Scale-Seven layout. The block instruments on Peter Denny's 'Buckingham' layout described. Construction review of the Model Signal Engineering 7mm scale kit for a LSWR lattice post signal. A follow-up to a previous article and correspondence on the public image of our hobby looks at public relations to make exhibitions pay.

"AUSTRALIAN MODEL RAILWAY MAGAZINE" April:

Drawing of a 4-8-0 locomotive thought to have been designed (and built surreptitiously) by the C.M.E. of the NSWGR in the early 1930s and subsequently lost from a ferry crossing the R. Clarence - (I think I smell a rat, this is the April issue! Branchline Editor)(Second aside..I would still like to find the real derivation of 'Wampu'..Managing Editor). Part Two of the article dealing with VR country station buildings of 1914 design (Part One was in the February 1993 issue) deals mainly with the various outbuildings, including the famous Australian outdoor dunnies - drawings and photographs abound. More on 'Balancing the Wagon Roster' emphasises the former abundance of open wagons. Repowering Lima diesel locomotives ('44 Class' and '422 Class' with Athearn SD40-2 mechanism, '44 Class' with K&M mechanism and '42 Class' with NWSL Power Drive trucks). Conversion of the Powerline '830 Class' of the SAR to 'Driver Only' for shunting operations drawings and lots of B&W and colour photos. A visit to "The Eyre Peninsula Division" garden railway modelled in 'In3 1/2', i.e. 3/8" to the foot (1:32) on 32mm gauge track (double 'Sn3 1/2') built over a period of 25 years. Building an automatic power shift driven by a separate motor for realistic miniature variation of cutoff! Paul Berntsen describes the etched side rods produced for the Berg's Hobbies model of the NSWGR 'X200 Class' rail tractor. Photographs which should have been with the article in the February issue on individualising the NSWGR GP and GC wagons. Underfloor detail for the NSWGR 'MRC' Refrigerator Car.

"MODEL RAILROADER"

February: 'MR'

Product Reviews' tells of the introduction of a range of 120 colours (90 for railroads) of Badger's AccuFlex acrylic paints with a dry film thickness of 0.001", available as gloss, semigloss and flat finishes. Also, at long last, a more or less half decent model of an oil refinery distillation column, heater, heat exchangers and pump alley from Walthers (does this impress our WA scribe, a retired BP employee? Managing Editor). Converting an Athearn 'HO' scale model of the EMD GP50 road switcher into a 'new image' Southern Pacific GP50, along with repowering with a Sagami motor. How to get 21 years of modelling satisfaction from a simple double track layout on a 4' x 8' board. Accurate period modelling, the example is that of the New York, New Haven and Hartford RR on Saturday, 3rd September 1948, with hints on research on the railroad and all sorts of things relative to the period (or date) chosen. Drawings, photographs and information on the K-6 Pacifics built in 1916 for the Chicago, Indianapolis & Louisville RR ("The Monon Route"). Making lenses for locomotive headlamps from clear casting resin. Using electrically conductive paint to light up figures (holding hand lamps, for instance), lamp posts and other details. Dave Frary starts on his modular, portable 'HO' scale model of the Pennsy and gives all the gen on making L-girders, the plywood trackbed and the risers - an article that should be read by everyone considering building an exhibition layout. Using surface mount resistors on axles of rolling stock to enable block occupancy detection. How to fit an Sn3 layout into a 14' x 12' room by 'double-decking' (15"-20" apart), a raised floor for the high part of the layout, a 'vertical' turnout and a two-turn helix - no space left for the bed though! Colour prints (ready to cut out) of three top billboard advertisements from 1948, 1949 and 1950 in 1:48, 1:64, 1:87 and 1:160 scales with 'formulae' for reduction by colour photocopier to 'Z', 'TT', 'OO' (British) and 'G' scales. 'Trains of Thought' ponders on the pluses and the minuses of multi-level layouts. Making an insulated drawbar between locomotive and tender. Using an Athearn GP38-2 for a South Shore Line loco, with hints on painting and 'scratch-building' the decals from various sources.

"CONTINENTAL MODELLER"

March:

"Saafedam Zee", an exhibitable 'HO' scale layout with an Austrian theme - article tells how to use 'sugar board' (thick card sandwich filled with dense polystyrene, which is available in the 'Free Box' in the Clubrooms) to create realistic stonework. "Barenburg/Castenada", an exhibition layout featuring both 'HO' and 'H0m' models in a slice of Switzerland. Brief story of the 'S' scale locomotives, scratch-built in tinplate, of the ECDTR (Dona Thereza Christina railway of Brazil), using tape recorder motors in the tender, universal drive and a sound system as well. Fine tuning of the 'flat can' motors fitted to Athearn 'HO' scale diesel locomotives. Using two different Lima 'O' scale locomotives to make a DB Kof shunter, an SBB Te2/2 shunter and a BLS electric locomotive. Drawings, photographs and information on a small 3'6" gauge 4-4-0T of the Gear Meat Co. of New Zealand. Modelling the sugar cane tramways of Fiji. "Steintal-

bahn V", described as 'Die Schmalspurbahn in HOe', is a purpose designed exhibition layout with good ideas for keeping viewers interested. Drawings, colour and B&W photographs and information on the six 43 Class Co-Cos built in 1956 for the NSWGR, together with a review of the Lloyds Model Railways' recently released 'HO' scale kit. Review of the Resistance Soldering System produced by Mignon Model Studios.

"MODELLERS' BACK TRACK"

December/January:

'Brigantes' (who is he?) writes on "Can we live with kits?", and suggests that the enormous range available may be doing the hobby a disservice. Part Three of the article on the GWR Dining and Restaurant Cars deals with the later Collett vehicles in 14 pages with B&W and colour photographs and descriptions and drawings for each of the Diagrams H41 to H57 inclusive. Drawing, photographs of prototype and model of a LNWR 30'1" Picnic Saloon built to Dia.84. Short essay with photographs confirming the extent of exchanges of tenders on the LMS. A 12 page article deals extensively with the 'homeless' LMS Class 5 ('Black Five') 4-6-0s and has a good drawing, many photographs and extensive text on the 225 locomotives built, as well as relating the trials of building the K's 4mm scale and the CCW 7mm scale sheet metal kit. Further research on Birkenhead (Woodside) to determine the platform allocations, the stock and the locomotives used 'on a summer day in 1934' (as derived and theorized from a 1934 LMS Working Timetable).

"MODELLERS' BACK TRACK"

February/March:

Indicating that there are many spin-offs from railway modelling, a detailed examination of the eight different styles of public telephone box that have been used by the (British) Post Office since their installation in 1921 - part of the research for the Birkenhead (Woodside) project. A nine page treatment of the passenger formations of the LNER, covering Ordinary Passenger Trains, Suburban Trains, Minor & Branch Line Trains. Drawing of the LNWR 2000 gallon 'Parachute' water column. B&W and colour photos of 10 different GNR signal boxes. Drawings and photograph of the MR's Whitwell (Derbys.) station building. Mainframes are examined, both prototype and model, to come up with eight modelling principles. Drawings, photographs and text covering the standard footbridge of the Highland Railway. Several photographs illustrating how signals were arranged/selected to fit difficult sighting locations. John Van Riemsdijk makes a case for Gauge 1 steam. Modelling of van clearance centre sidings (miniature marshalling yards for maximising the usage of vans) with some useful modelling hints on vans on the model railway.

"MODEL RAILWAY"

COLLECTOR" Issue Four:

Reviewing the many producers of Welsh narrow gauge railways in 16mm/ft scale running on 32mm gauge track. Collecting Trix Twin Railways on a budget. The importance of models' boxes to collectors. A look-back at the pre-war Hornby Gauge O No.3 locomotives with the 4-4-2 wheel arrangement but representing "Royal Scot" 4-6-0, "Flying Scotsman" 4-6-2, "Lord Nelson" 4-6-0 and "Caerphilly Castle" 4-6-0! Another look-back, this time to the Hornby

MO, Brimtoy, Mettoy and Chad Valley CV 0-4-0 locos from the interwar years. Dry-brushing to get an effective weathered effect. Overview of the model railway collecting scene in the U.S.A. The hazards of taking commissions for the building of locomotive and other kits and complete railways.

"MODEL RAILWAY"

"COLLECTOR" issue Five:

The Kitmaster 'Blue Pullman' coaches described and a table giving very useful information on the numbering and the make-up of the "Midland Pullman" 6-car sets and the "South Wales Pullman" 8-car sets. A look-back at the pre-war Hornby Gauge O No.2 locomotives with the 4-4-0 wheel arrangement. Brake vans in 'OO' scale produced by Bing, Trix Twin, Liliput, Hornby Dublo, Wrenn and Tri-ang Hornby Railways. A "Buyer's Guide" to the Tri-ang Hornby 'OO' scale LMS "Princess" Class 4-6-2 locomotives - four pages detailing the values of the various versions of this model over the years. Lionel Trains of the immediate post war era. A more detailed look at the rivals to Hornby Gauge O, namely Mettoy, Wells-Brimtoy and Marx.

"MODEL RAILWAY"

"LOCOMOTIVES" No.7

(bound into "Model Railway Collector" Issue Four): This issue continues in the same style as the six previous Parts and covers the following types of locomotives, both the 4mm and 2mm scale models that have been available as proprietary models over recent years: GWR 2251 Class 0-6-0, SR 'West Country' and 'Battle of Britain' Class 4-6-2 (unrebuilt and rebuilt versions), LNER J72 Class 0-6-OT, BR Class 56 diesel electric locomotive, BR Class 81 electric locomotive and LNER V2 Class 2-6-2.

"MODEL RAILWAY"

"LOCOMOTIVES" No . 8

(bound into "Model Railway Collector" Issue Five): As with the previous issues, this issue looks at the 4mm and the 2mm scale models issued by the proprietary manufacturers over recent years of the following": GWR 43XX Class 2-6-0, BR(W) 'Western' Class diesel hydraulic locomotive, GWR 'Dean Goods' 0-6-0, BR Class 45 diesel electric locomotive and the BR Metro-Cammell DMUs.

"THE CLEARING HOUSE"

March:

Review of the INFOCOM Command Control System, made in Australia. A layout design inspired by Callington (LSWR) in Devon. Design and dimensions for 4mm scale of a general purpose mid-Victorian warehouse, a cut down version of one of the buildings in a superb diorama featured in the April 1983 issue of "Railway Modeller". Super colour photograph of the loco coaling stage on Harry Howell's 'OO' scale "Stafford" layout. Harry 'dredges up' Bob Essery's article in the September 1963 issue of "Railway Modeller" on coaling up of locomotives.

"MODEL RAIL" No. 189:

48 page Supplement (No.22) to "Rail" No.189 - the biggest to date: Mendip Models range of solders described and a recently released white metal 4mm scale kit of a BR 24 ton MTV Tippler Wagon. Review of the Dapol 'OO' scale model of the BR Class 155 'Sprinter'. Tabulation of what models of BR diesel locomotives are available, discontinued or proposed, for both 'OO' and 'N' scales. Review of the Ratio Oil Depot kit, useful for the steam/diesel transition era too. Using normal rub-on Letraset Helvetica medium to produce

your own station nameboard signs. Facing the problems of modelling the international traffic after the Channel Tunnel opens - the dual problems of space (partially overcome by the Solent MRG by modular design, compatible with modules from European model railway clubs) and the old, old question of modelling scale differences (it's inevitable that the British modellers of international traffic will have to do it in 'HO' scale: British 'OO' and British 'N' are incompatible with the European models). A suggested layout based on Barry station in South Wales. "Caron Road", an 'EM' gauge 8' x 1'8" exhibition layout (which normally lives in the loft) based on the former Caledonian Railway in more modern times and featuring a number of 'pointers' to good presentation at exhibitions. Colour drawings of BR Class 90s in French (SNCF), Belgian (SNCB) and German (DB) liveries for the Freight connection 92 Exhibition in Birmingham - transfers were made at short notice by Fox Transfers (Barry Bryant has recently donated a Fox Transfers catalogue to the Library). Dave Lowery takes over Modern Motive Power and explains the construction of a BR Class 142 DMU 'Pacer' in 'O' scale. The re-painting and in-house transfer service offered by Replica Railways together with their mahogany presentation display cases.

"MODEL RAIL" No.191:

Photograph of the Hurst Models 4mm scale ZZV snowplough kit. Adding realism to colour light signals. Looking at the BR 20 ton Brake Vans in 4mm scale from Lima, Replica, Dapol and Hornby and how to improve the Hornby version (deemed to be the best of the four!).

Extracts supplied by Ted Thoday
and Alan Porter .

Brian is sorting out magazines at ~ Train World

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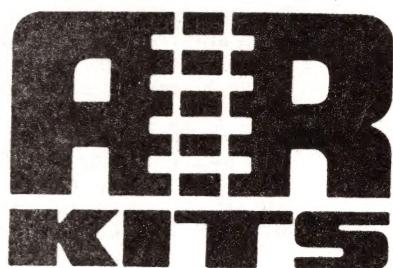
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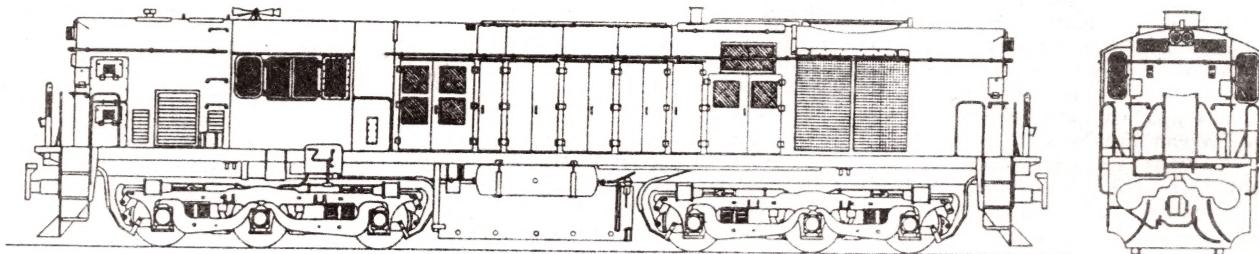
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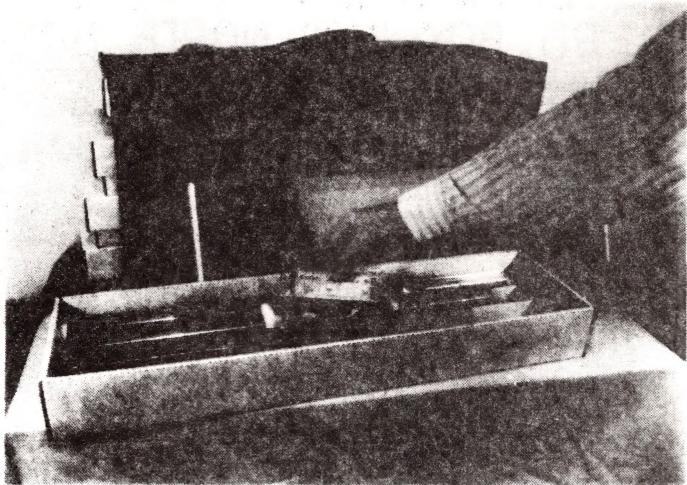
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